





SECTION III  
**TYPOLOGIES**

# AREAS OUTSIDE OF STUDY

The following section attempts to divide and categorise Barnet's urban environment in accordance with the layered approach described in the methodology. The first stage in this process is to identify areas outside of the study.

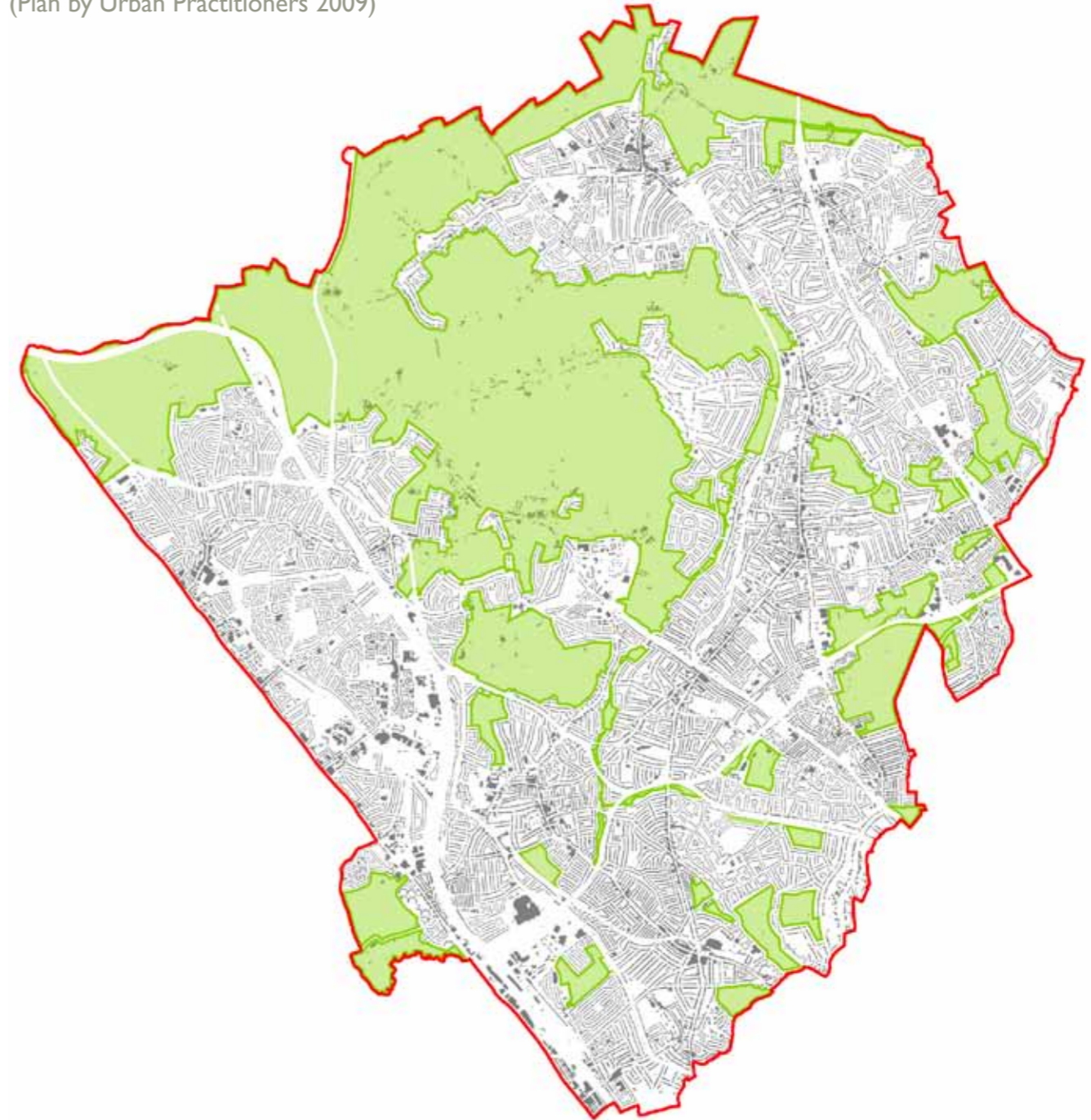
## GREEN SPACE

The first group of areas that fall outside of the study area includes designated areas of Green Space. These include Green Belt, Metropolitan Open Land, cemeteries, allotments, local parks and nature conservation areas (for more information on each designation please see Green Space section). These areas are already protected by rigid constraints they are protected by policy and therefore resistant to inappropriate development.



*Approximately one third of the Borough is designated Green Belt*

**Green Space**  
(Plan by Urban Practitioners 2009)



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# AREAS OUTSIDE OF STUDY

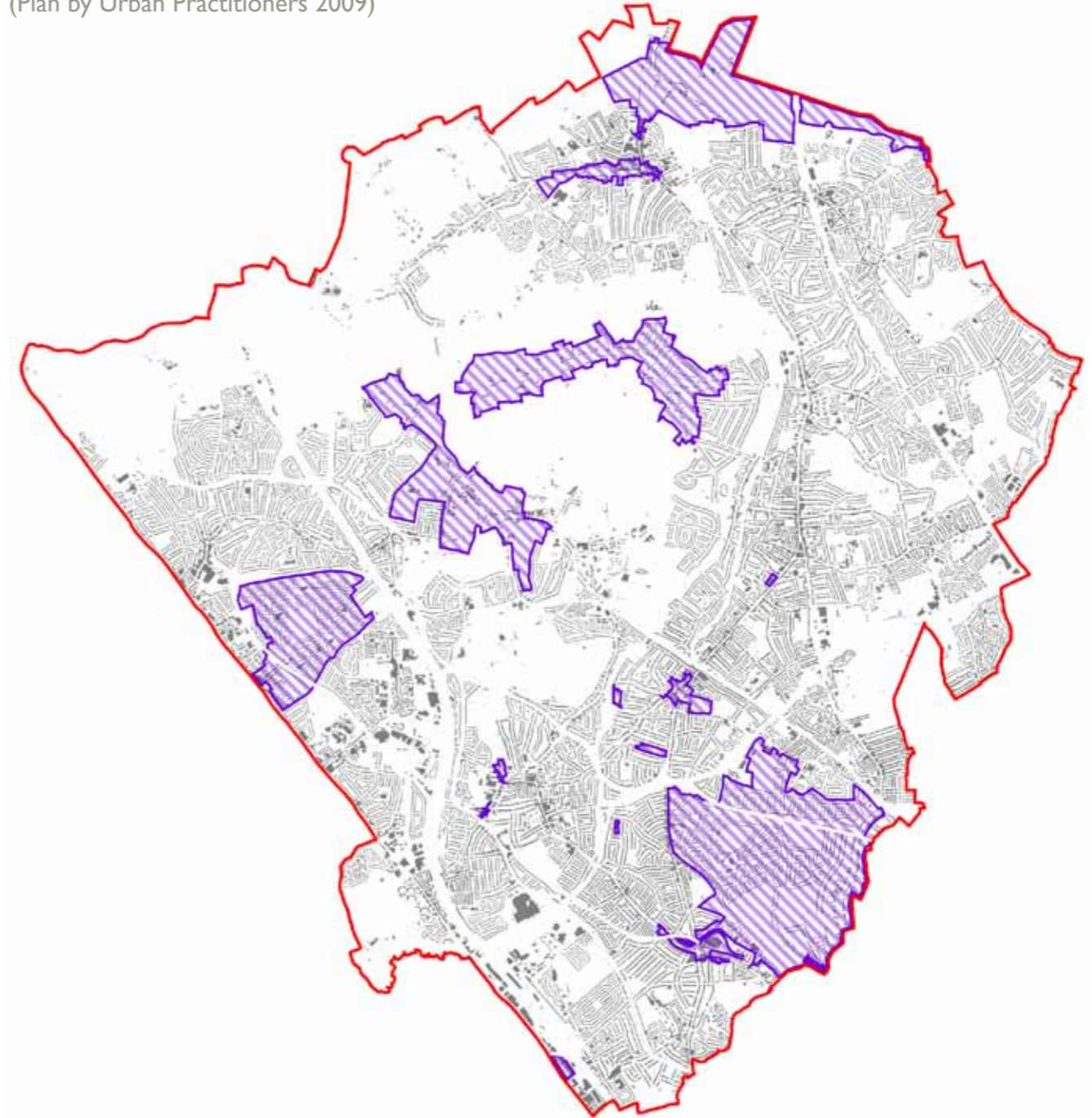
## CONSERVATION AREAS

The second group of areas identified includes designated Conservation Areas across the borough. These areas already have coherent character and consequently have are protected by policy and therefore resistant to inappropriate development. For a comprehensive list of conservation areas throughout the borough please refer to the Planning Policy section located in Appendix II.



Located in the south east of the Borough, Hampstead Garden Suburb is Barnet's largest Conservation Area

**Conservation Areas**  
(Plan by Urban Practitioners 2009)



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# AREAS OUTSIDE OF STUDY

## MAJOR GROWTH AREAS

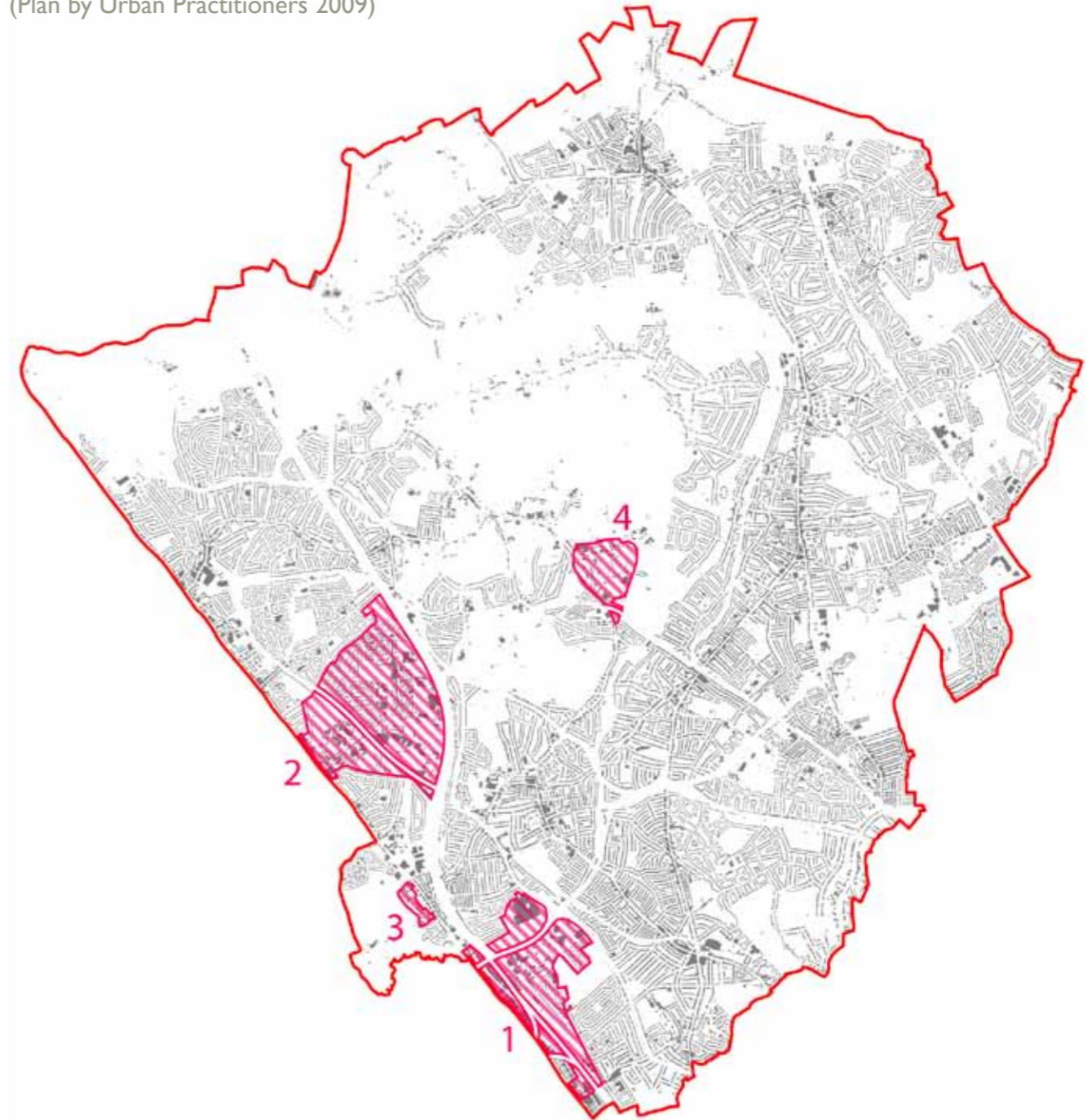
The third and final group identifies areas of major growth. These specific areas have been identified as appropriate locations for change, and are therefore subject to controlled and managed development. Major development is proposed at the following locations:

1. Brent Cross and Cricklewood
2. Colindale
3. West Hendon
4. Mill Hill East



*Beaufort Park is located in Colindale, and has been identified as a major growth area*

## Major Development Areas (Plan by Urban Practitioners 2009)



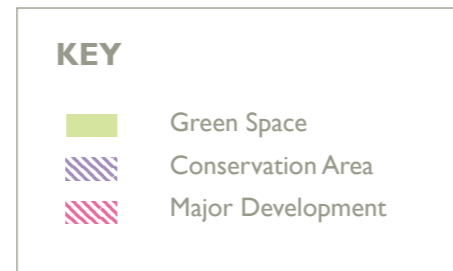
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# AREAS OUTSIDE OF STUDY

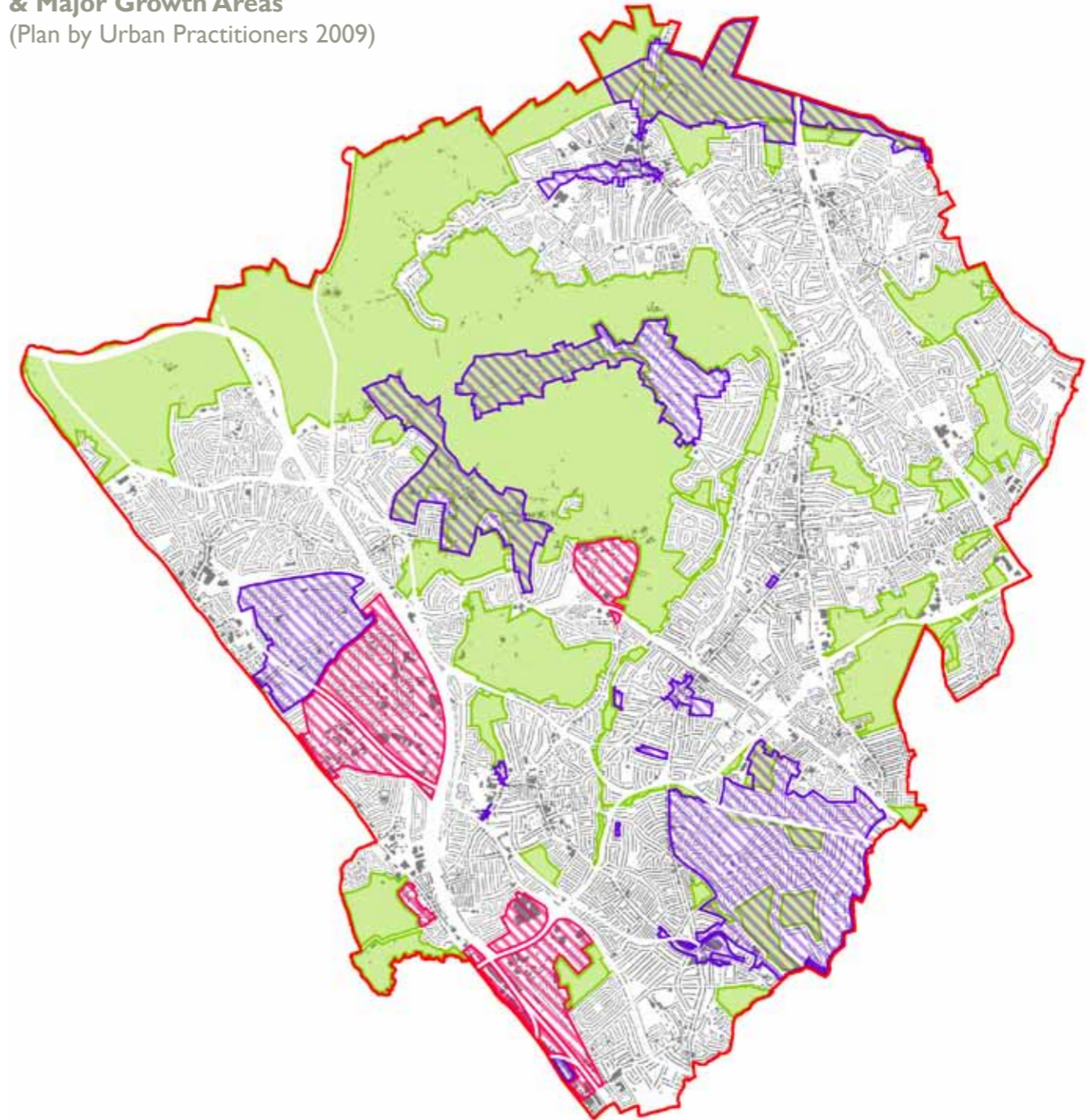
## SUMMARY

By showing these three layers together, the remaining urban area becomes visible. This area is relatively unprotected and requires further analysis in order to establish areas that can accommodate future growth and those that require safeguarding.

These urban areas are those which, whilst protected by planning policies do not have the same high level of protection as conservation areas or the same planned change as the major growth areas.



**Green Space, Conservation Areas  
& Major Growth Areas**  
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# PRIMARY TYPOLOGIES

The remaining urban area will now be broken down and categorised in order to be effectively analysed. The criteria used to distinguish between street typologies includes the following:

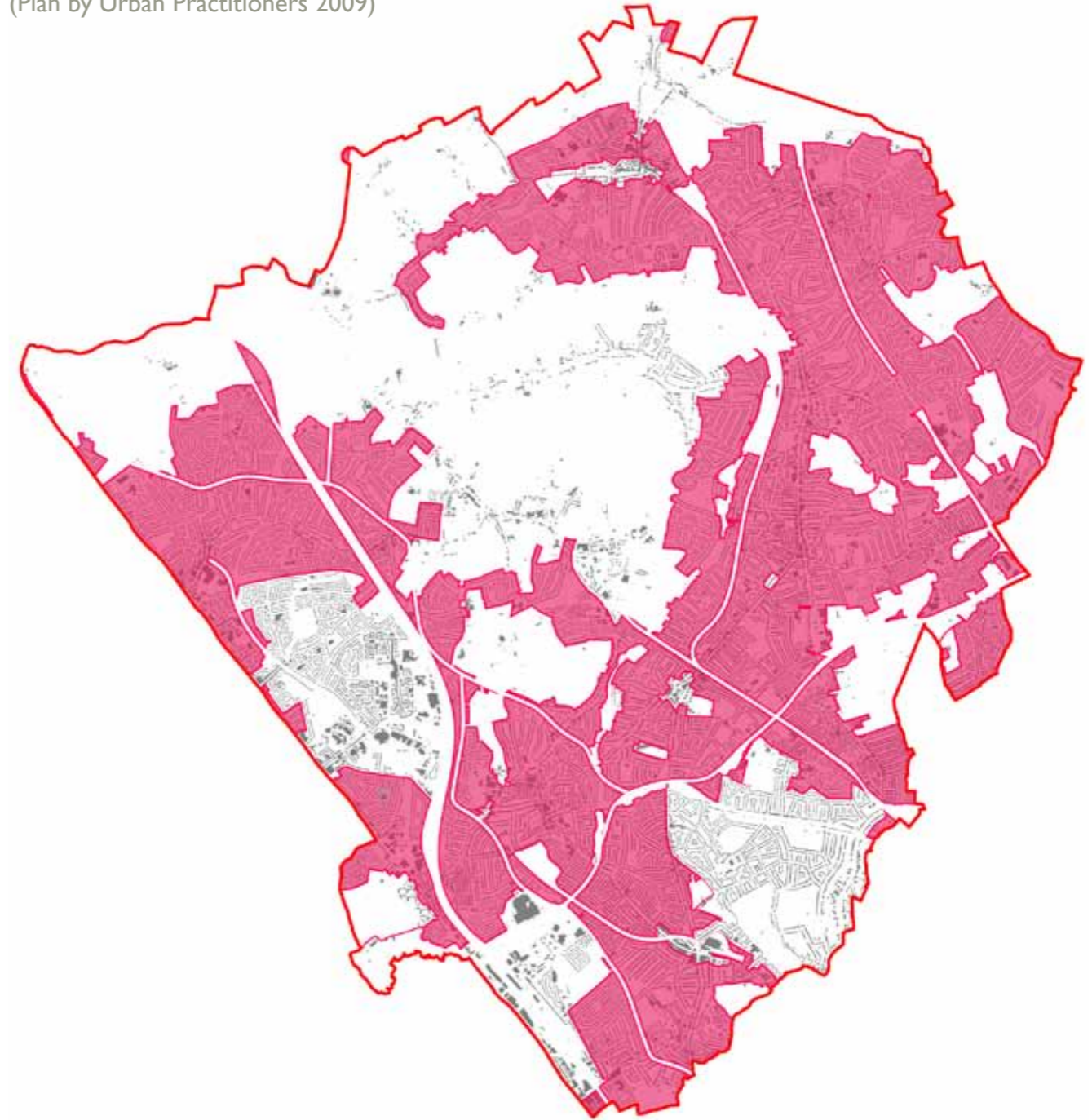
- Scale and grain – the pattern, size and arrangement of buildings and their plots;
- Land use – the predominant function of the buildings;
- Network characteristics – the arrangement of vehicular roads and pedestrian paths, and their relationship with surrounding buildings.

The application of these three criteria has identified five different primary street typologies, including -

- Box development;
- Campus Development;
- Cores and Town Centres;
- Residential Estates; and
- Residential Streets

Details of each typology are illustrated on the following pages.

**Remaining Urban Area**  
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# BOX DEVELOPMENT

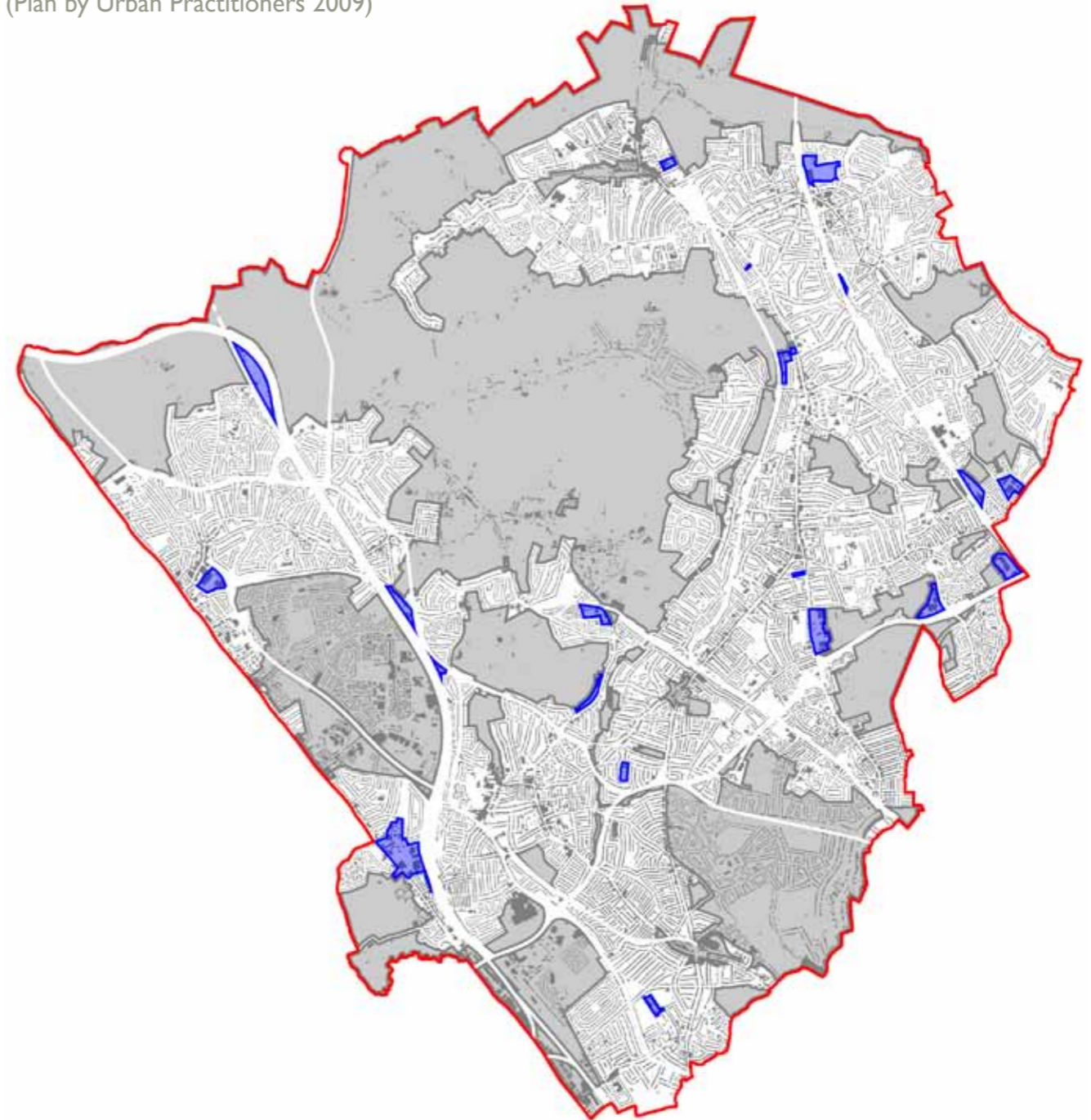
Box development is most typically large scale retail units such as supermarkets, or industrial development. It is most frequently located in close proximity to large infrastructure and a significant amount borders the M1 motorway, the North Circular Road, and rail lines. Occasionally box development is located in and around town centres, where the historic grain has been redeveloped to accommodate large scale retail units.

Both the Colindale AAP and Brent Cross and Cricklewood Masterplan contain a significant amount of urban development that meet the criteria of box development. However, as these locations have already been identified as areas scheduled for controlled and managed redevelopment these are not identified on the plan.



*Box development occupies a range of land uses, including retail uses, and is typically surrounded in car parking*

**Box Development**  
(Plan by Urban Practitioners 2009)



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# BOX DEVELOPMENT

## *Scale and Grain*

Box development is primarily distinguished by its large urban scale and grain, as buildings have exceptionally large floor plans making a clear distinction from any adjoining development. Individual buildings typically have little or no relation to surrounding streets and are usually arranged to suit commercial and operational requirements, rather than to conform to any urban design criteria.

## *Land Use*

This typology occupies a range of land uses, including retail, industrial and commercial uses.

## *Network Characteristics*

Box development is typically organised around series of car park and cul-de-sacs, accessed via a feeder road, and is therefore non-permeable for both pedestrian and vehicular movement. Buildings are often surrounded in space dedicated for car parking, with little or no provision for pedestrian movement between units.



*Located in Colindale, the Hyde Estate is a typical example of box development*

# CAMPUS DEVELOPMENT

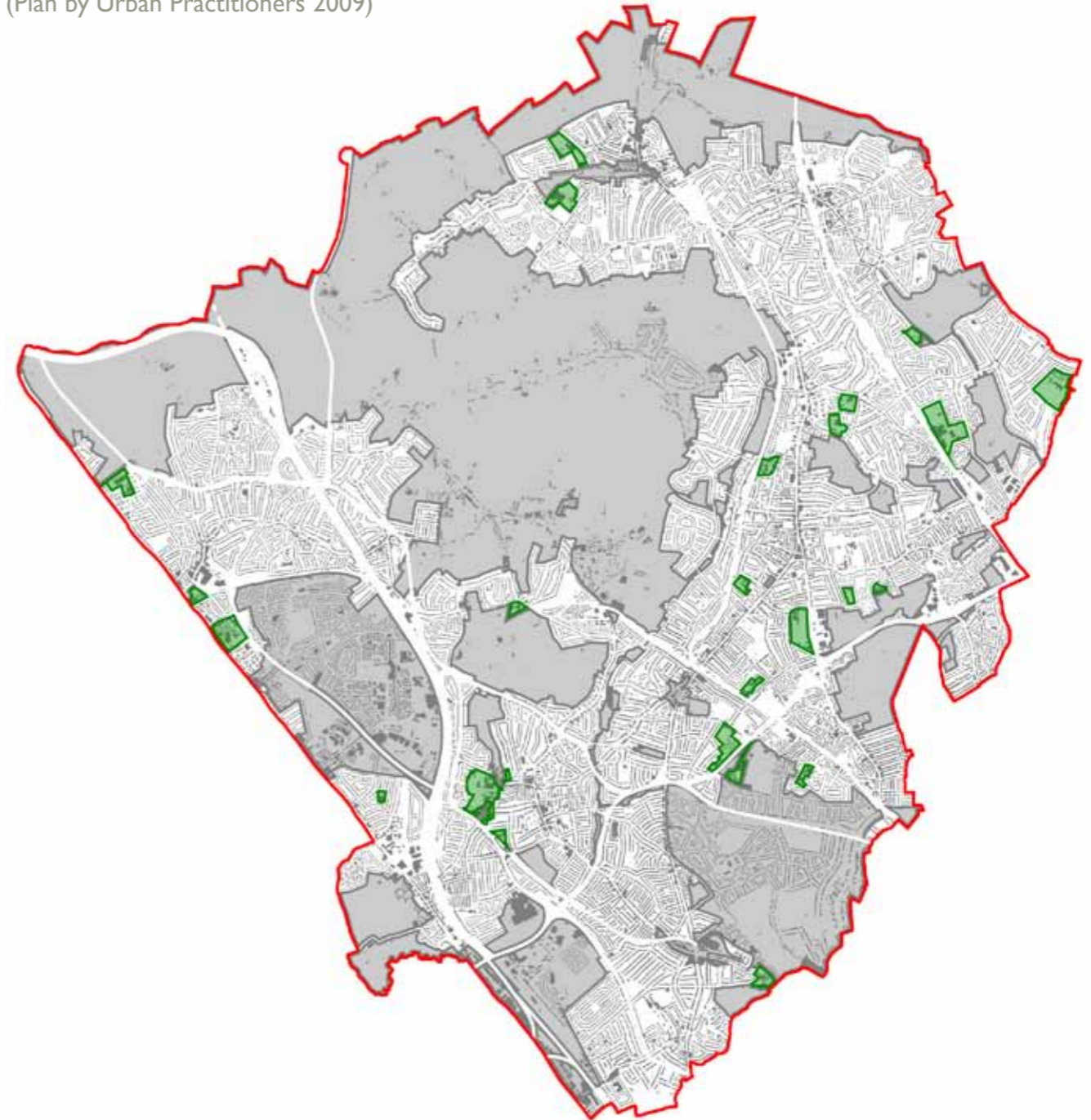
This typology occupies a range of uses across the Borough, including colleges, hospitals, civic buildings and business parks. Campus development typically comprises large building or groups of buildings, and can be distinguished from Box development through the greater emphasis placed on surrounding landscape and the pedestrian environment. Furthermore, they tend to follow a cellular form on several floors, as oppose to providing large single spaces such as large box retail uses.

Notable examples of campus development in Barnet include Middlesex University, Barnet College and Barnet General Hospital.



*Middlesex University is a notable example of campus development*

**Campus Development**  
(Plan by Urban Practitioners 2009)



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# CAMPUS DEVELOPMENT

## *Scale and Grain*

This typology is also primarily distinguished by its large urban scale and coarse grain, as buildings have large floor plans.

## *Land Use*

Campus development typically accommodates a range of educational, civic and leisure uses.

## *Network Characteristics*

Campus development typically segregates vehicular and pedestrian movement, but is often characterised by a lack of through routes connecting the development to adjacent areas. Some examples allow the car to dominate, with pedestrians relegated to secondary routes. Older examples of campus development, including established civic and educational institutions, provide a good quality pedestrian environment and have a stronger relationship with the street.



*Middlesex University is a typical example of campus development where large buildings are set in landscaped surroundings.*

# CORES AND TOWN CENTRES

The London Borough of Barnet contains several areas that have been identified as town centres. This analysis reflects the urban character of the areas, and therefore the boundaries defined are distinct from those outlined in planning policy designation. Many town of the present centres originated from villages, and are principally located on higher ground, reflecting the original settlement pattern.

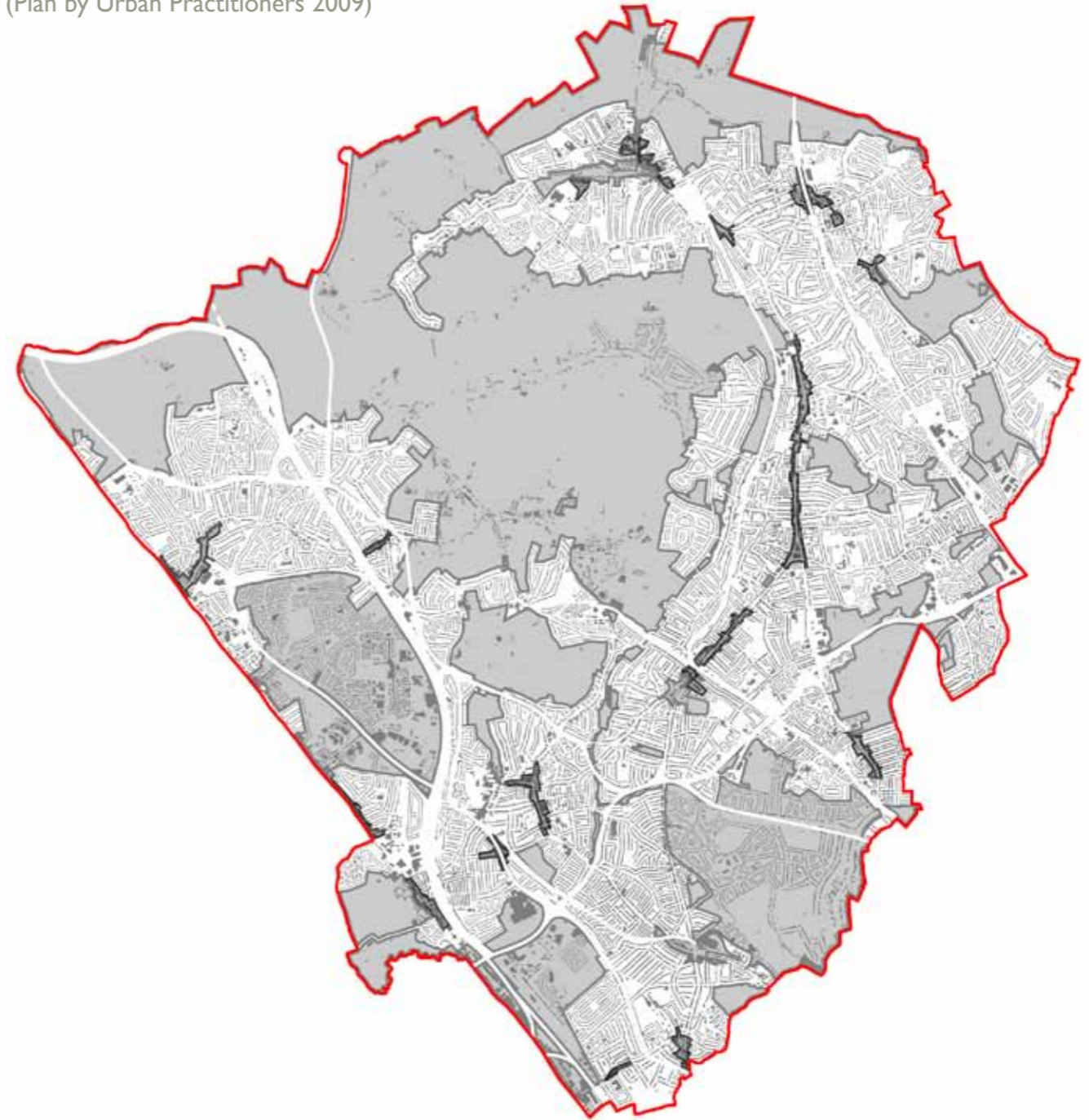
These areas are often located along major vehicular routes traffic has had a significant impact upon their historic character. Ribbon development along these routes has resulted in elongated areas of town centre activity. The impact of modern highway design has also impacted upon the public realm of such areas, as many are now dominated by vehicular movement.

Aside from the historic village centres there are a number of urban centres, many of which are the product of early twentieth century planning and respond to rapid growth of the urban area. These tend to have a single, cohesive character and strong overall composition.



Town centre retail uses

**Cores and Town Centres**  
(Plan by Urban Practitioners 2009)



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# CORES AND TOWN CENTRES

## *Scale and Grain*

Many town centres have originated from historic centres, and typically contain some of the oldest parts of urban fabric through the Borough. This is reflected in the typology's scale, which tends to be of a fine grain. Buildings are organised facing on to streets. Later twentieth century centres such as Hendon tend to have a larger and more regular form, but are still orientated around key routes and public spaces.

## *Land Use*

Town centres are most easily distinguished from surrounding areas by its land use. Although retail uses predominate, such areas incorporate a range of other functions including civic, commercial, leisure and residential uses.

## *Network Characteristics*

Town centres are permeable as pedestrian and vehicular movements follow the same routes.



*The impact of modern traffic onto the town centre at New Barnet is significant, as the urban environment is dominated by vehicular movement.*

# RESIDENTIAL ESTATES

Residential Estates are primarily distinguished from Residential Streets by the way the buildings are organised, and how they relate to streets and open spaces.

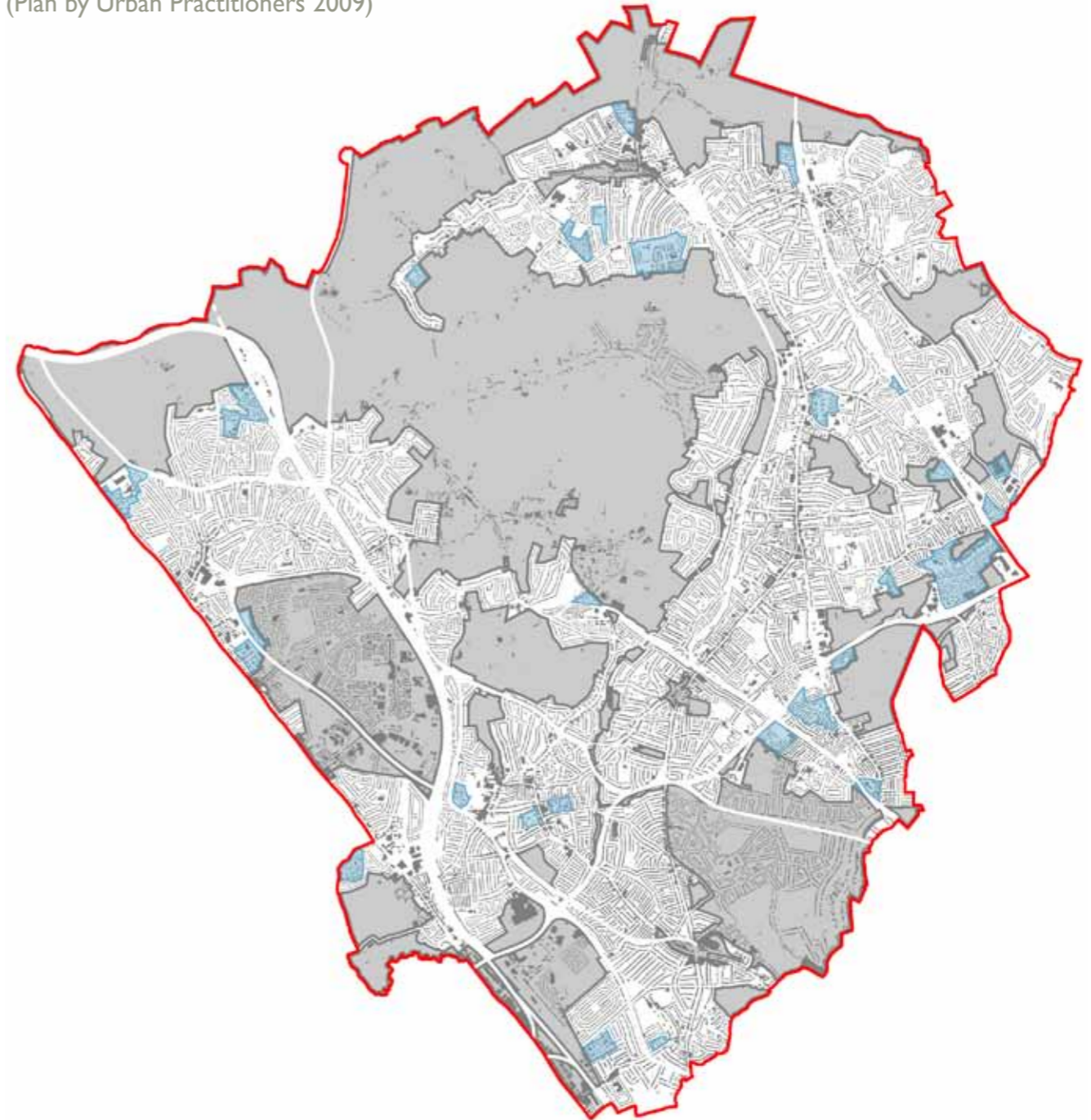
Postwar housing is the most prominent example of the conventional estate. Such development marked a radical departure from the earlier suburban development typical of the interwar period. Instead of conventional perimeter block structures which create clear networks of streets many new estates were designed around complicated layouts which by separating pedestrian and vehicular movement created an illegible environment. The confusing layouts and the lack of active frontages onto conventional streets can make even key thoroughfares feel poorly overlooked and unsafe.

There are also a number of recent estate developments which follow the suburban model of cul-de-sac development. These estates are characterised by the lack of clear public realm, standardised housing types and the over-dominance of the car.



*Dollis Valley is a typical post war estate, where segregation of vehicular and pedestrian movement contributes to the bleak urban environment*

**Estates**  
(Plan by Urban Practitioners 2009)



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# RESIDENTIAL ESTATES

## *Scale and Grain*

Buildings have floor plans that are of similar scale to traditional residential urban development. However unlike traditional urban environments, buildings within residential estates are not exclusively arranged around streets. In many cases houses are set in landscaped surroundings, and are accessed via a series of often complex pedestrian pathways.

## *Land Use*

Land use within estates is almost entirely residential, with occasional retail units.

## *Network Characteristics*

Unlike traditional residential developments, houses within estates are often accessed via courtyards and open spaces, as oppose to directly from the street. Another key feature is the segregation of pedestrian and vehicular movement, as estates are characterised by a series of alley ways and narrow pedestrian routes. Although this provides a very permeable layout, it does not tend to create a legible or safe environment.



*Unlike traditional residential development, estates are characterised by unorthodox and often complex urban layouts, as pedestrian movement is confined to a series of confusing and often illegible paths.*



# RESIDENTIAL STREETS

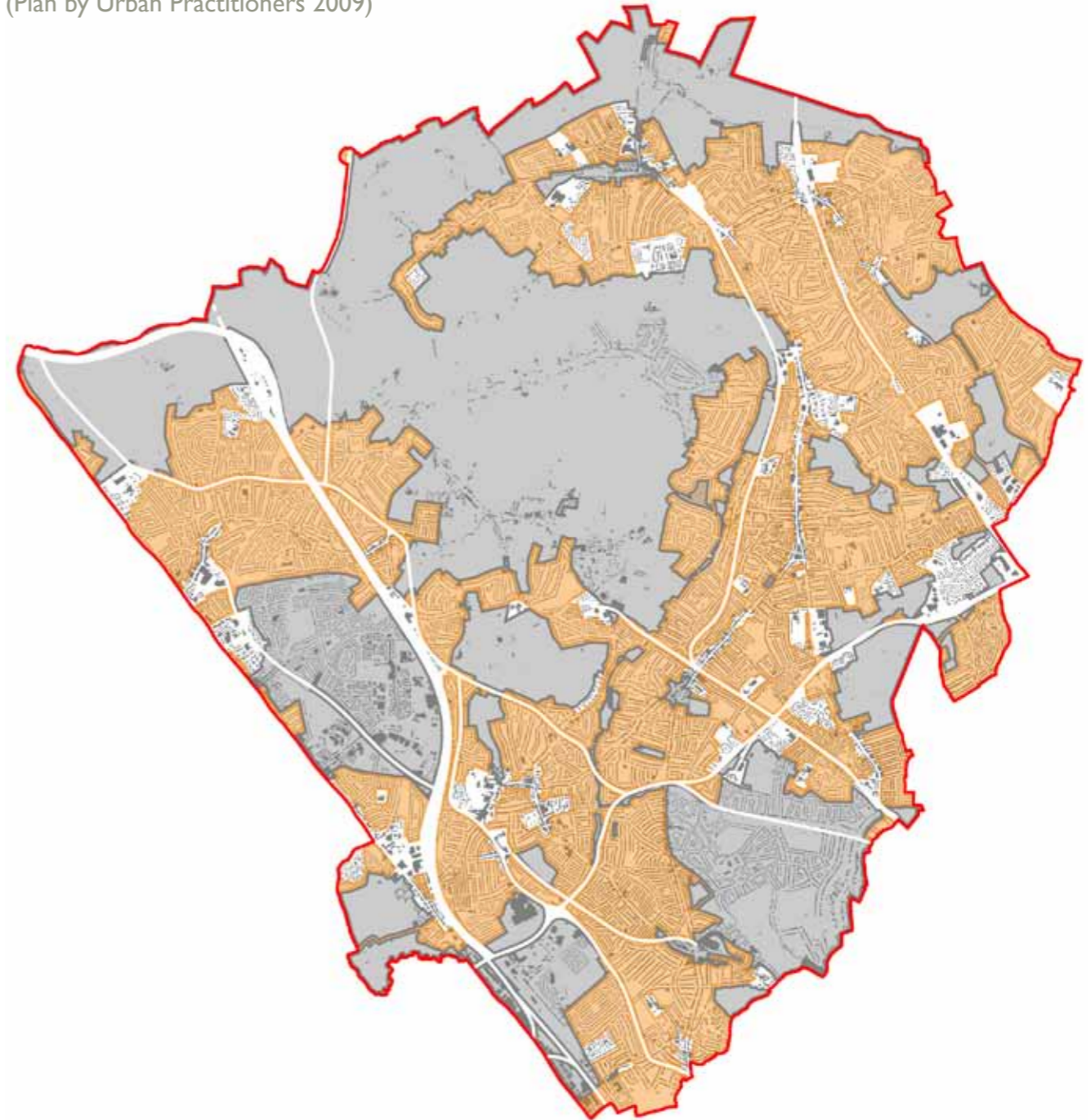
Conventional residential streets are the predominant form of development in Barnet, and most often associated with interwar housing. They are characterised by a simple loose grid of streets forming a permeable network, with each plot having frontage directly on the street.

Within this typology there are several variations that have a range of physical characteristics. In order to further distinguish between the many different types of residential streets that make up the Borough it is necessary to break down this primary typology into secondary typologies. These secondary typologies are described in greater detail in the follow section.



*Suburban residential development is the predominant urban character in Barnet.*

**Residential Streets**  
(Plan by Urban Practitioners 2009)



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# RESIDENTIAL STREETS

## *Scale and Grain*

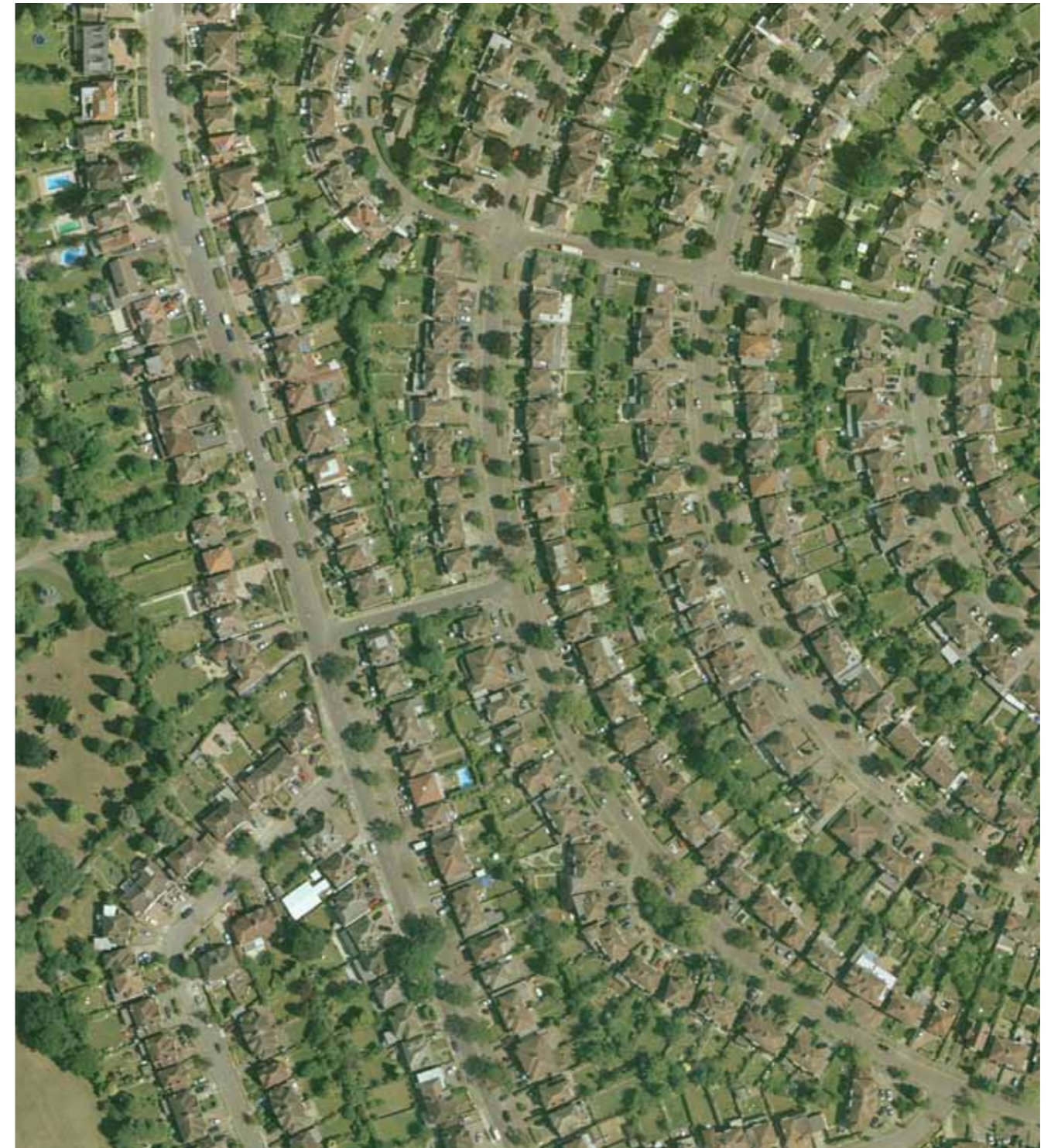
The nature of most residential streets in Barnet is to provide regular plots with individual dwellings. This domestic scale and repetitive modular approach is a key feature of these streets.

## *Land Use*

Land use within this primary typology is predominately residential, however in some locations there are small retail and commercial functions, such as local parades of shops.

## *Network Characteristics*











Residential streets are generally organised into a permeable grid structure, providing an integrated network of pedestrian and vehicular routes.



*Residential streets in Barnet are typically organised as a loose urban grid with strong built frontages*

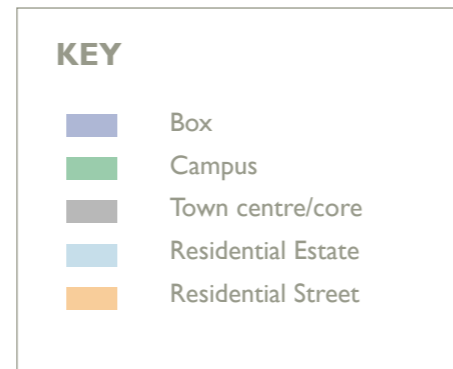
# SUMMARY

The table below provides a summary of the primary typologies, and is analysed against the criteria used to define each typology.

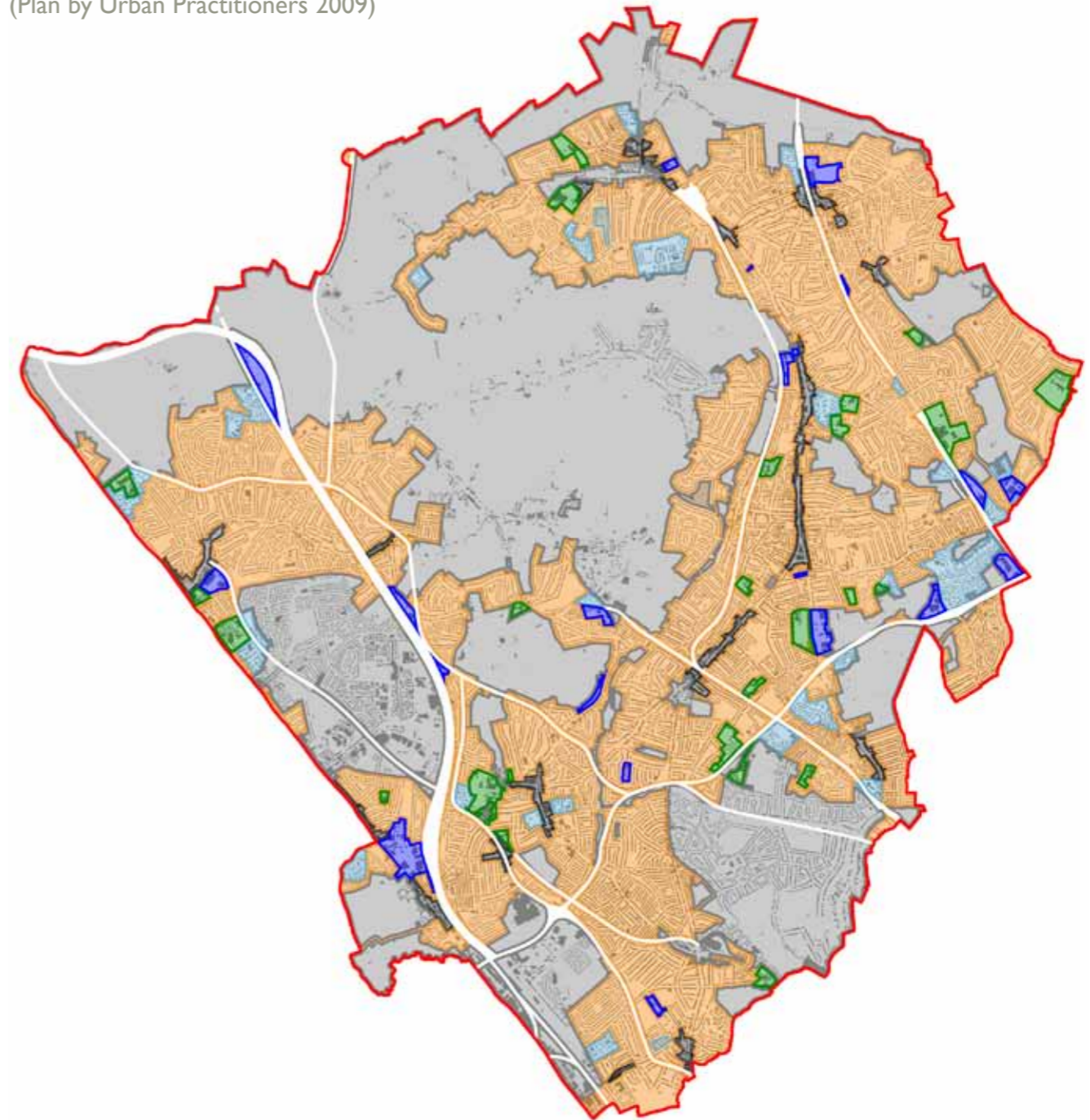
	Photograph	Aerial	Scale and Grain	Land Use	Network Characteristics
<b>Big Box</b>			Large single building units (sheds) typically coarse grained and without an over-arching urban structure. Often surrounded in parking.	Industrial, retail, and leisure	Non permeable, typically unsuited for pedestrian movement.
<b>Campus</b>			Large building units set in landscaped open space	Education (universities and secondary schools), civic, business, office, hospitals and leisure.	Non permeable with limited pedestrian connectivity.
<b>Cores and Town Centres</b>			Medium-to-large building units arranged along streets forming strong terraces and coherent forms.	Mixed land use including retail, civic, residential, and office.	Permeable grid, based around strong street frontages and high levels of activity. Very well suited to pedestrian movement.
<b>Residential Estates</b>			Variable building scales, set in landscape and/or parking	Residential	Fine grain network of pedestrian routes, with a distinct lack of clear structure, hierarchy and legibility.
<b>Residential Streets</b>			Small-to-medium building units arranged along streets. Urban blocks tend to be large.	Residential	Permeable grid, although the scale of urban blocks limits the pedestrian connectivity.

# SUMMARY

The plan illustrates how each of the primary typologies relate to one another. The largest proportion of the study area has been identified as residential streets. In order to gain a better understanding of how urban character varies across these streets, this primary typology is split into a series of secondary typologies.



**Primary Typologies**  
(Plan by Urban Practitioners 2009)



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# SECONDARY TYPOLOGIES

The following criteria have been adopted in order to categorise the residential streets typology:

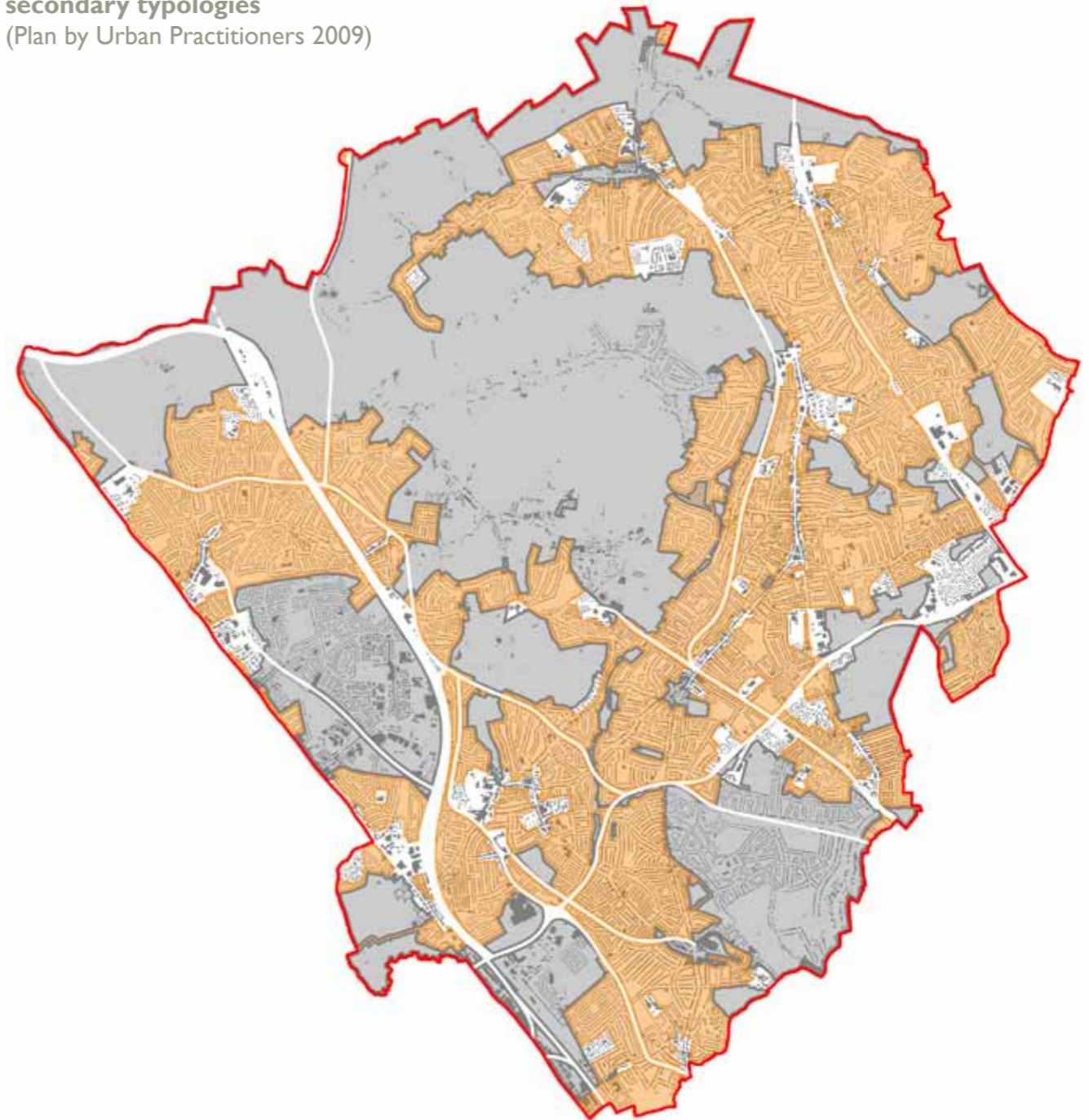
- Development density;
- Building types and units;
- Enclosure, street width, setbacks and building height;
- Architectural style and period; and
- Landscape character and streetscape

The application of these five criteria has identified six secondary typologies, which include the following:

- Type A - Linear rural
- Type B - Suburban periphery
- Type C - Suburban
- Type D - Suburban terrace
- Type E - Urban terrace
- Type F - Flats

Details of each typology are illustrated on the following pages.

**Residential Streets to be divided into secondary typologies**  
(Plan by Urban Practitioners 2009)



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# A - LINEAR RURAL

## Introduction

Linear rural development is the product of sporadic, piecemeal growth along country lanes or tracks, which are often later widened or straightened to take more traffic yet retain their informal character. The most obvious example in the Borough is Barnet Road, which meanders from east to west from Whalebone Park to Barnet Gate. There is no consistent house type or period and materials and styles are equally diverse. Although the more recent houses tend to be on narrower plots the planting – and especially trees and hedges – are the main contributors to the character of the area.

Totteridge Lane is a noteworthy example of a Linear rural route, however as it is a designated Conservation Area it is not included in this part of the study.

## Density

Linear rural routes are distinctly low density forms of development, as the net density varies from 2 -10 dwellings per hectare.

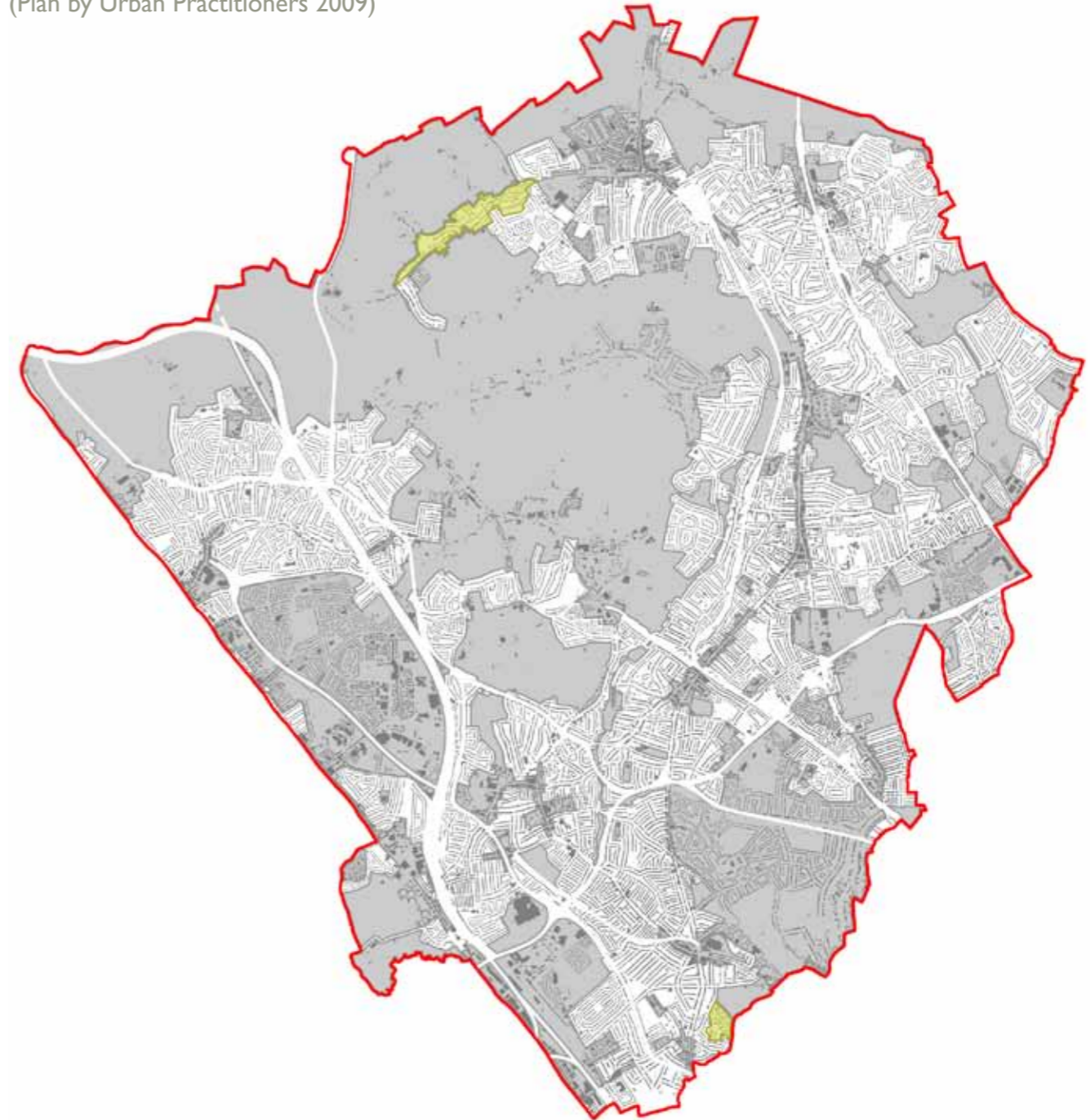
## Building Types and Units

Houses are almost entirely detached from one another, as building heights range from 1 – 3 storeys in height.



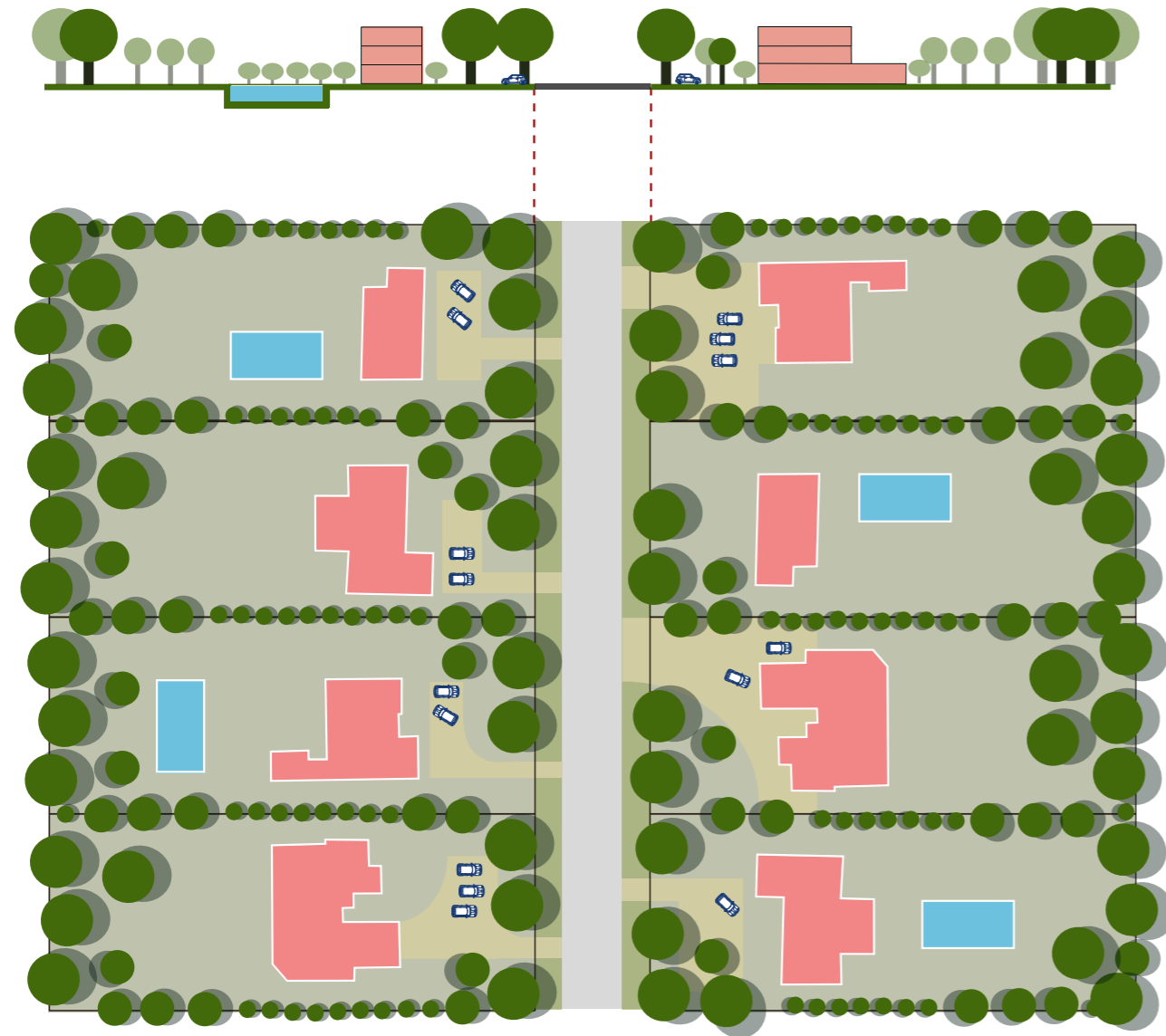
Parking is well integrated into front gardens, as vegetation primarily encloses the street space

## Linear Rural Streets (Plan by Urban Practitioners 2009)



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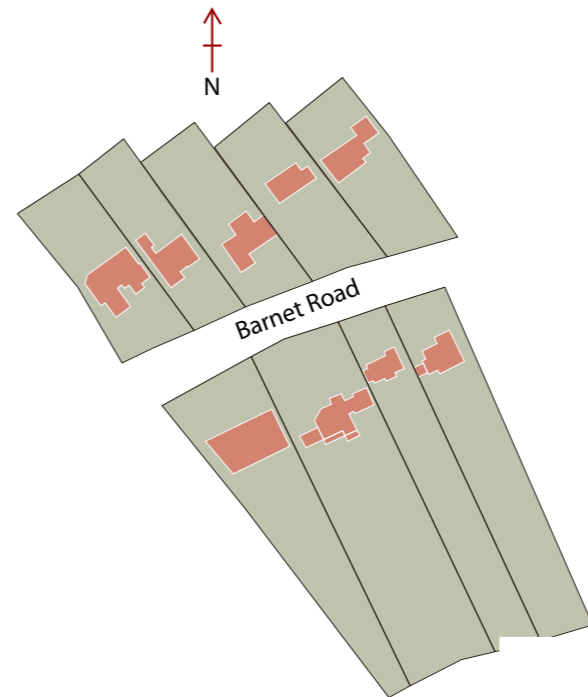
# A - LINEAR RURAL



Street Width  
15 - 20 M

Set - Back  
8 - 40 M

Plot Depth  
50 - 120 M



Plot Width  
16 - 32 M



# A - LINEAR RURAL

## Enclosure

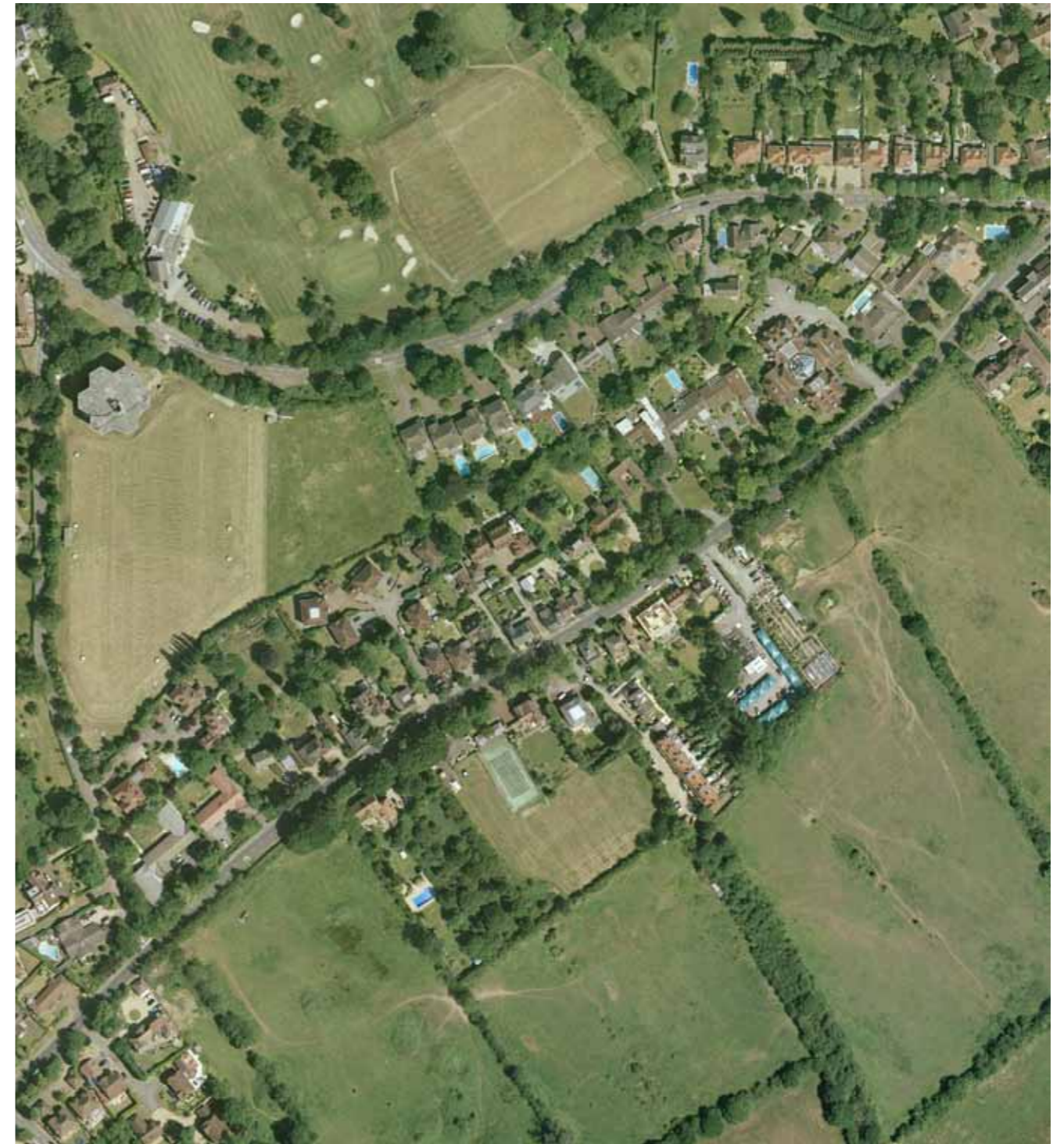
One of the primary distinguishing factors of linear rural routes includes the manner by which the street space is enclosed. Unlike the vast majority of the Borough's residential streets, where the public realm is defined by residential buildings, the public space along linear rural routes remains primarily enclosed by mature vegetation, including trees and hedgerows. The historic lanes themselves tend to feature narrow carriageways, sometimes with wide grass verges on either side. Carriageways are typically no more than 6 metres in width, however the wide grass verges give the street space a typical width of 15 – 20 metres. The dense belt of trees and vegetation that lines the street space grants a high level of privacy and exclusivity to the properties, a key characteristic of this secondary typology.

Additionally, linear rural streets are characterised by exceptionally large plots. Building fronts can be set back from the plot edge by up to 30 metres, providing copious amounts of space for on plot parking; it is not uncommon for houses to accommodate in excess of four vehicles on plot.



Figure ground plan of typology

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Aerial view of typology

# A - LINEAR RURAL

## *Architectural Style and Period*

Linear rural routes are also distinguished from other residential streets by the complete lack of any consistent architectural period or style. These lanes have historically featured development of a range of sizes. However, with the gradually escalating exclusiveness of these addresses there has been significant pressure to redevelop and renew, resulting in some areas of very large and sometimes flamboyant modern properties mingled in with the few historic properties which survive.

## *Landscape Character and Streetscape*

The streetscape of the linear rural routes is typically leafy and rural in character. These are characterized by mature native species hedgerows on both sides, grass verges (no pavements or road kerbs) and little or no street lighting. The road is occasionally bordered by fields and hedgerow and through which there are occasional glimpses to the landscape beyond. Vegetation is typically mature native species (predominantly oak, ash and hawthorn) with a wider variety of species in private gardens.

Streetscape materials are typically macadam roads, the larger ones of which are edged with concrete kerbs and have standard highways street lighting. There is no on street parking and there are few public pavements, instead grass verges line the road.



*In many cases along Barnet Road mature trees predate houses*



*Detached properties along Barnet Road are indicated from the road by distinctive curved white wooden marker posts indicating the property name.*

# B - SUBURBAN PERIPHERY

## Introduction

A considerable amount of streets through out the Borough have been identified as the suburban periphery typology.

The most notable characteristics of this typology include the large plot sizes, and sense of enclosure provided by on street vegetation. A key physical element that distinguishes suburban periphery streets from linear rural routes is the wider street space in the former. These streets are typically mature suburban areas rather than former rural lanes. Almost all areas have been identified in the north of the Borough, with the largest single area identified east of Totteridge.

## Density

Due to the exceptionally large building plots and detached units that line them, suburban periphery streets are characteristically low density. Typical net density varies from 10 - 15 dwellings per hectare.



Typical suburban periphery housing with large set back and on plot parking

## Building Types and Units

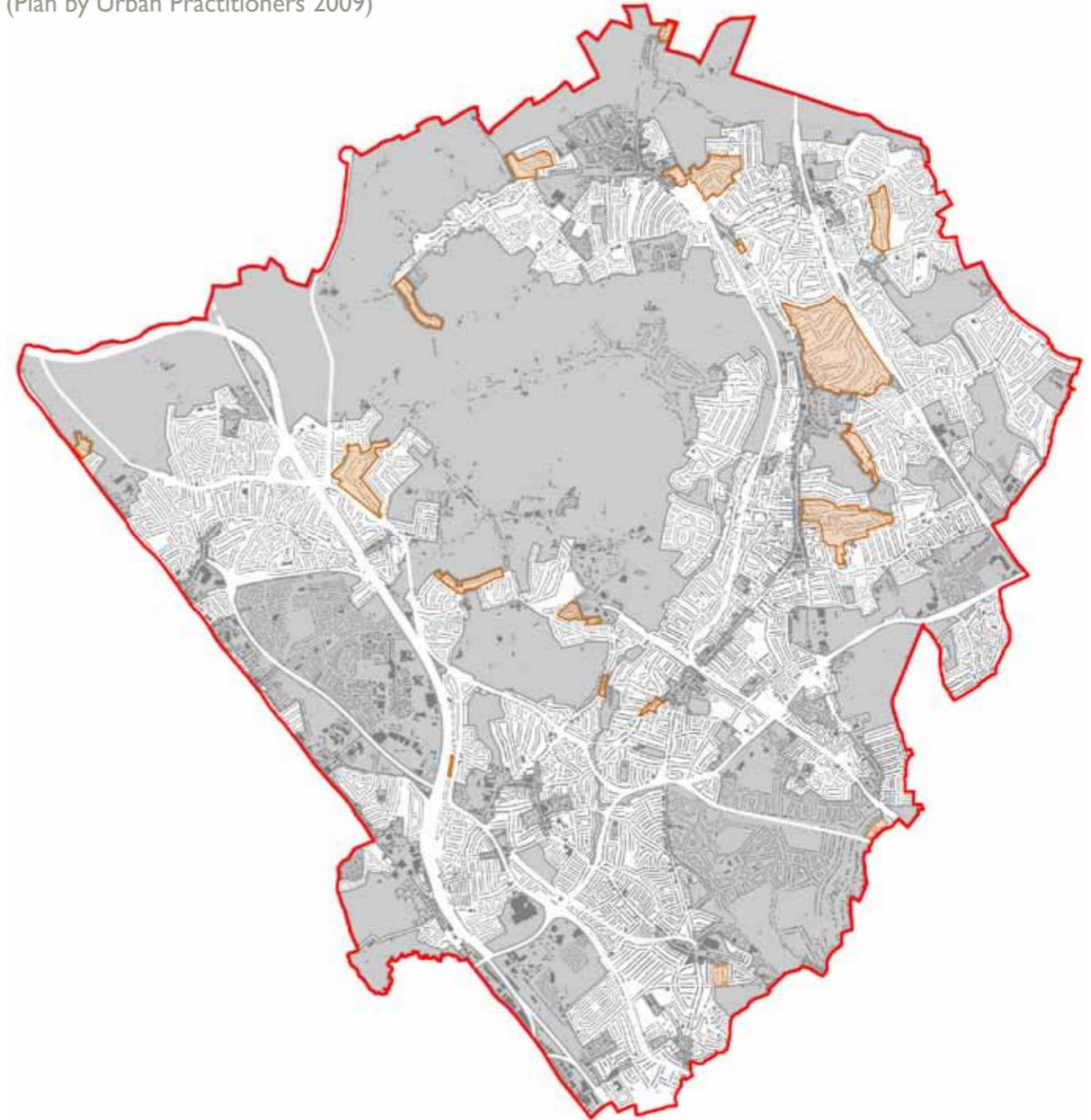
Suburban periphery streets almost entirely contain detached houses which vary from 2 - 3 storeys in height.

## Enclosure

The suburban periphery typology is enclosed primarily by vegetation, including mature trees and hedges. The streets space is generally wide and typically ranges between 11 and 13 metres in width.

Plot sizes are typically large, and as buildings are very well set back they only provide a secondary form of enclosure. The distance between building fronts and the plot edge ranges between 8 and 14 metres. Plot sizes can differ in width and depth, but generally there will be a common relationship with the street and planting, at least in the public realm, is often orderly. Due to their exceptional set back, houses can often accommodate parking for several vehicles on plot.

## Suburban Periphery Streets (Plan by Urban Practitioners 2009)



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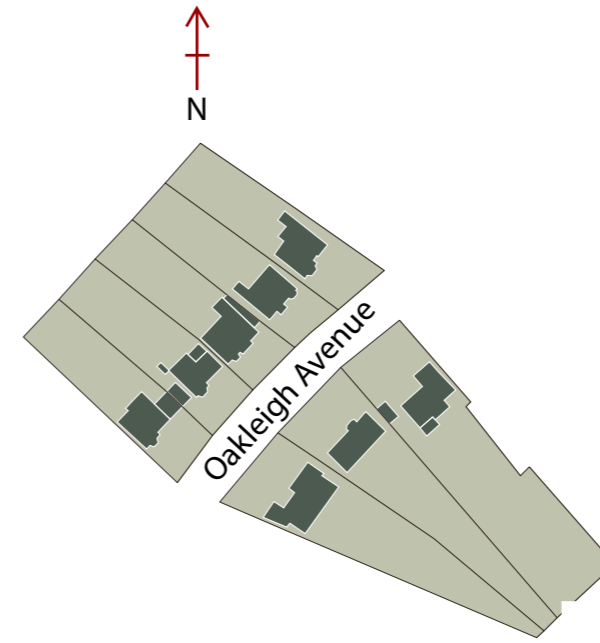
# B - SUBURBAN PERIPHERY



Street Width  
11 -13 M

Set - Back  
8 -14 M

Plot Depth  
50- -80 M



Plot Width  
15 -23 M



# B - SUBURBAN PERIPHERY

## *Architectural Style and Period*

Houses are typically the product of the early twentieth century, and in particular the interwar years. In a fashion not unlike linear rural routes, areas of suburban periphery development have no stylistic consistency. Properties which can be found in suburban periphery streets can include any neo-Georgian, mock-Tudor, arts and crafts rustic or even international modern. Materials can be equally diverse, although red brick and white render are common.

In some cases however, there are ribbons of large detached houses built to a common template and plot dimensions; these generally date from the inter-war period and have a greater formality than elsewhere.

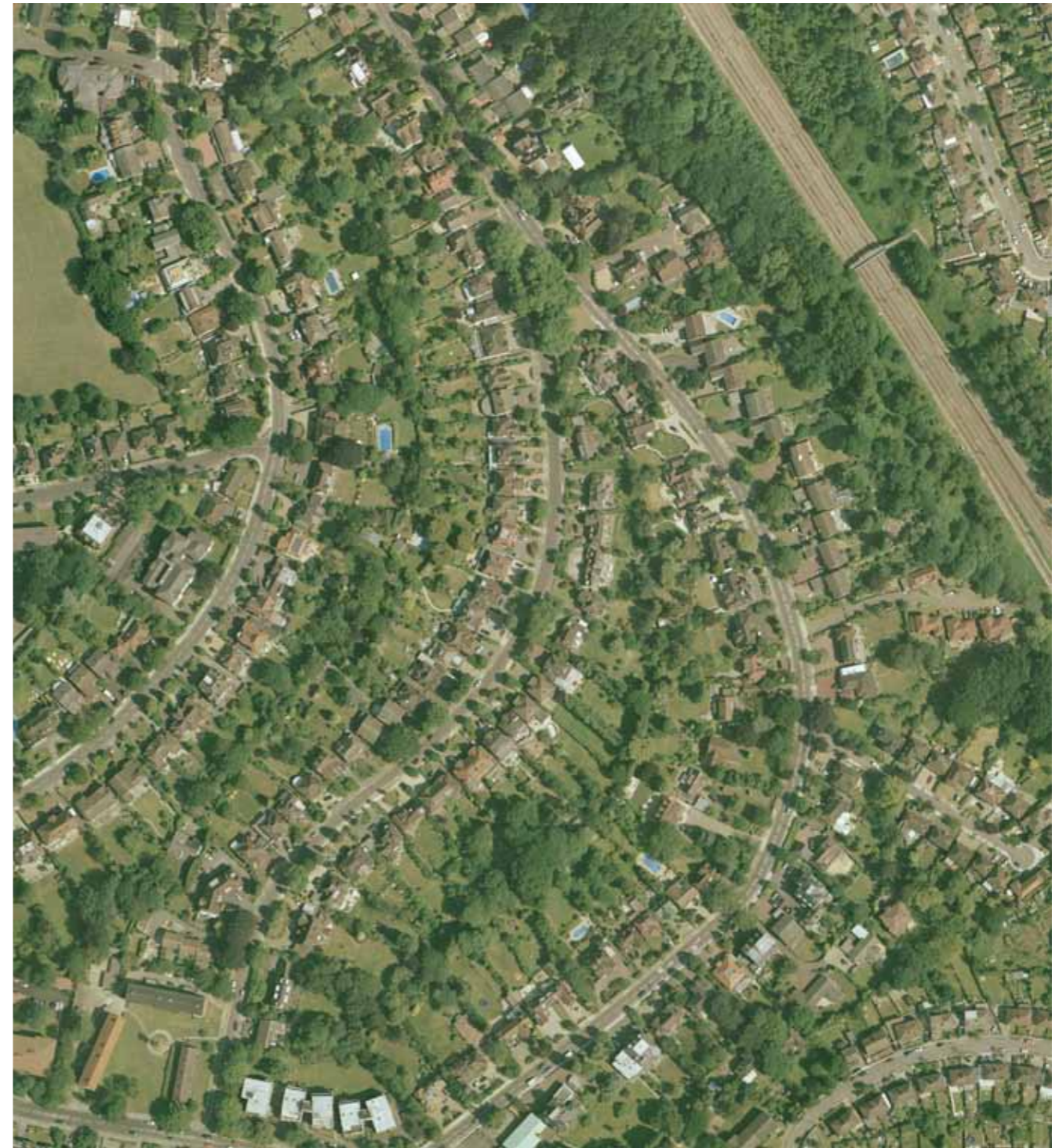


*Figure ground plan of typology*

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*Houses are typically between two and three storeys*



*Aerial view of typology*

# B - SUBURBAN PERIPHERY

Increased number of vehicle cross overs reduces the continuity of the verge

Wide plots and deep gardens enable retention of planting and front boundary treatments whilst accommodating on-plot parking

Mature planting plays a significant role in defining the character of the street

Some modern infill schemes lack the dynamic roofline characteristic of the original houses



Photographic elevation of typical suburban periphery

## Landscape Character and Streetscape

The areas of suburban periphery housing typically have a very wide street profile (up to 40m between building fronts) and a leafy and exclusive character. Detached houses are set well back from the road with substantial mature front gardens normally incorporating a private drive.

Houses have been designed with car ownership in mind, and parking is typically well integrated into the front gardens and/or in garages. The generous building set back allows front gardens to accommodate a range of vegetation in addition to several car parking spaces. A significant proportion of front gardens remains devoted to planting, as mature trees and hedges make a considerable contribution to the overall leafy character of suburban periphery streets. They

are typically open plan and visible from the street with low walls defining the front boundary. Most are well-maintained with a range of ornamental tree and shrub species and grass lawn areas. Some incorporate large mature trees which pre-date the houses. Some roads include street tree planting set in a narrow grass verge although these are often small, ornamental trees and trees in front gardens are often more substantial.

Streetscape materials are typically macadam roads with a granite kerb and pavements surfaced in pre-cast concrete slabs or macadam. Private drives are surfaced in a variety of materials ranging from stone 'crazy paving' to modern concrete block and brick paving. Street furniture and lighting is mainly late twentieth century standard highway fittings.



There is a clear boundary between public and private space



Parking is well integrated on plot

# C - SUBURBAN

## Introduction

The defining physical characteristic of suburban streets is the low density coupled with an overall level of architectural coherence. The majority of suburban streets in Barnet are lined with houses built during the interwar period. This was a time of rapid growth in Barnet and it is the period of development which most strongly typifies the Borough. Many streets benefit from a strong sense of architectural consistency and coherence, as houses typically have been built to very similar specifications, often by a single developer. Additionally this secondary typology can be distinguished by its large street space widths and lack of enclosure.

## Density

The density of suburban streets is a marked increase from both linear rural streets and suburban periphery streets, ranging from 20 – 30 dwellings per hectare.

## Building Types and Units

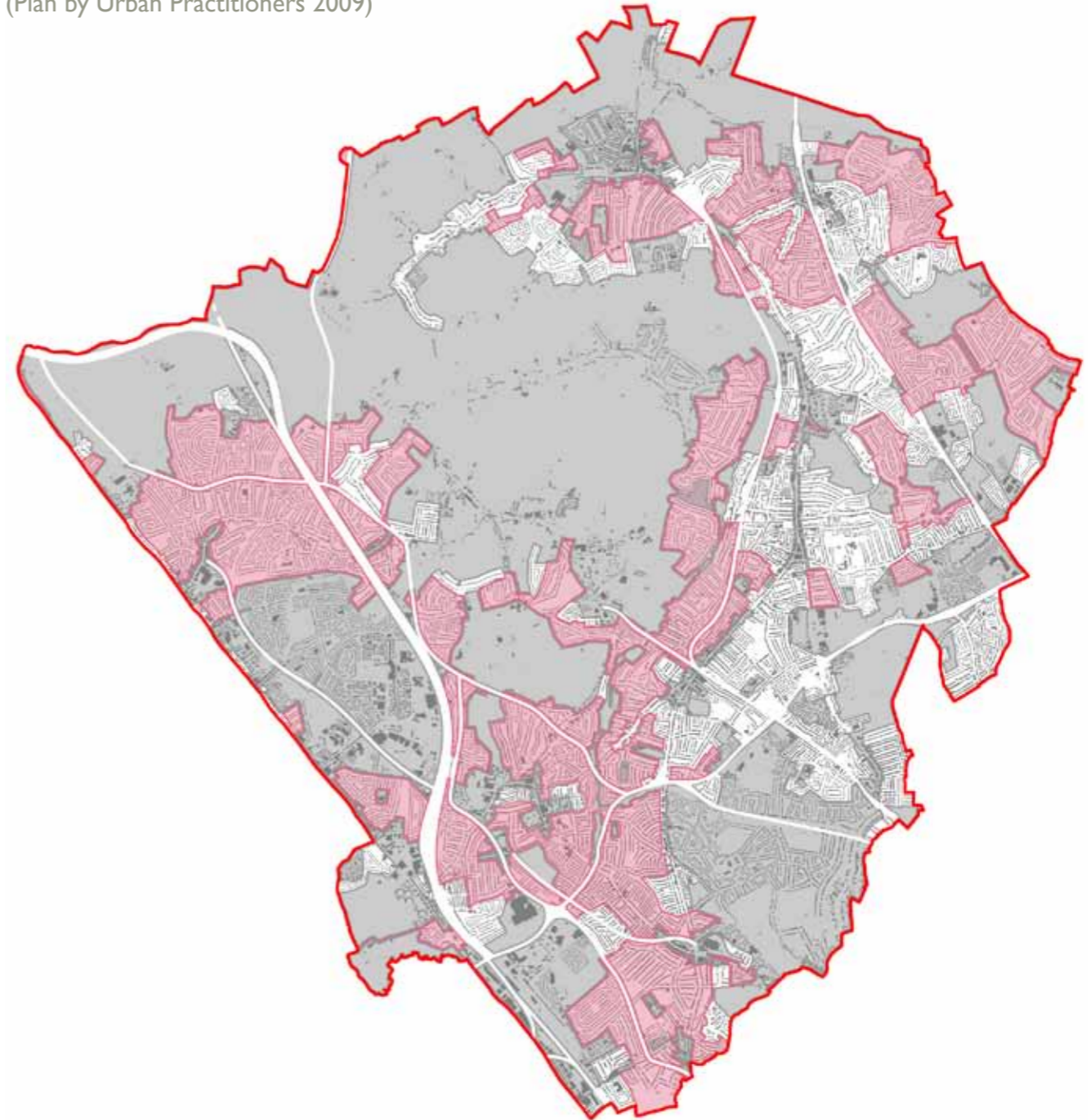
Suburban streets are lined with both detached and semi detached houses. Building heights are predominantly two storeys, with the occasional three storey house where conversions have occurred.

## Enclosure

As opposed to linear rural and suburban periphery streets where carriageways and street space is primarily enclosed by vegetation, building fronts provide almost all enclosure on suburban streets. Street widths tend to vary between 12 – 18 metres, as pavements are often generously wide. Suburban streets contain an array of detached or semi detached houses in very close proximity to one another. Typically, building fronts remain well set back from the plot edge, as distances range from 5 – 14 metres. Consequently, suburban streets can often accommodate a significant amount of on plot parking.

## Suburban Streets

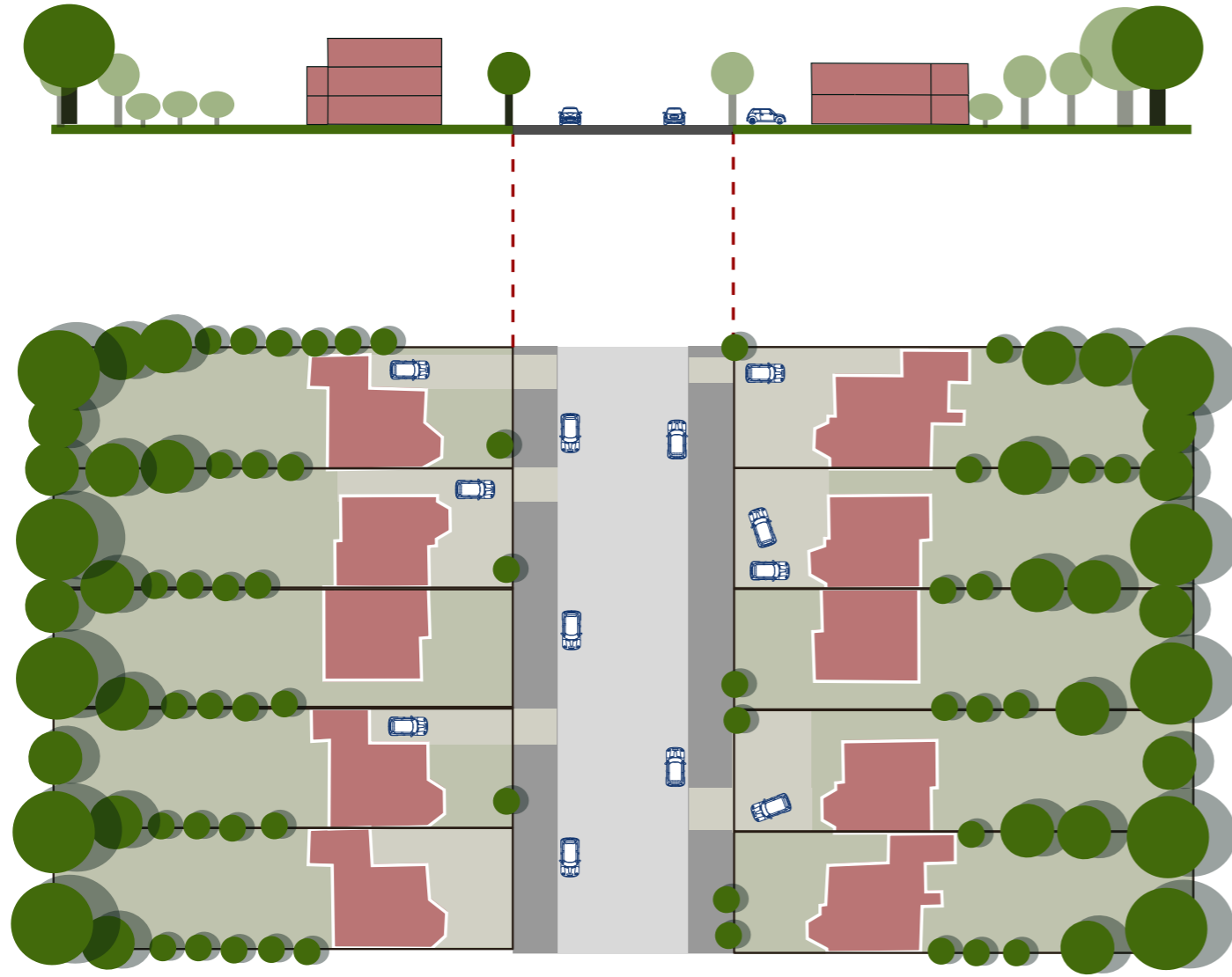
(Plan by Urban Practitioners 2009)



*The majority of front gardens have been converted hard standing surfaces to accommodate car parking*

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# C - SUBURBAN



Street Width

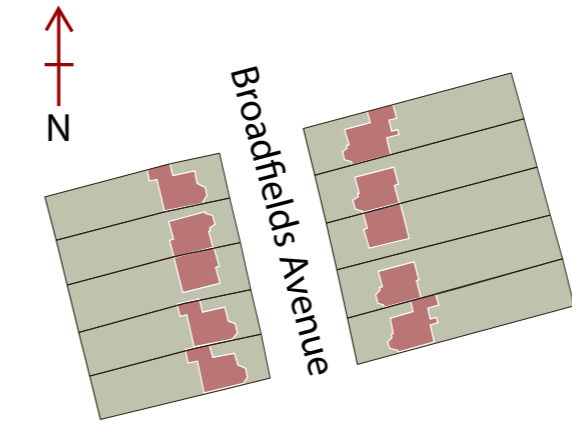
12 - 18 M

Set Back

4 - 9 M

Plot Depth

38 - 45 M



Plot Width  
8 - 12M





# C - SUBURBAN

## *Architectural Style and Period*

A key distinguishing characteristic of suburban streets remains the dominance of housing built during the interwar period. Buildings of this period typically display a clear lineage from their Edwardian predecessors, but with an increasing emphasis towards horizontal rather than vertical proportions. Houses of this period typically feature side-hung or top-hung windows rather than sliding sash.

A wide palette of materials may be found including red and yellow brick, render, pebbledash; timber or metal windows, hardwood or softwood doors. In the past twenty years UPVC replacements have been commonplace, and in some instances this can be done successfully without damaging the overall appearance. Plain tile roofs are often replaced by interlocking concrete tiles. In some instances glazed green pantiles survive, but these are now a rarity.

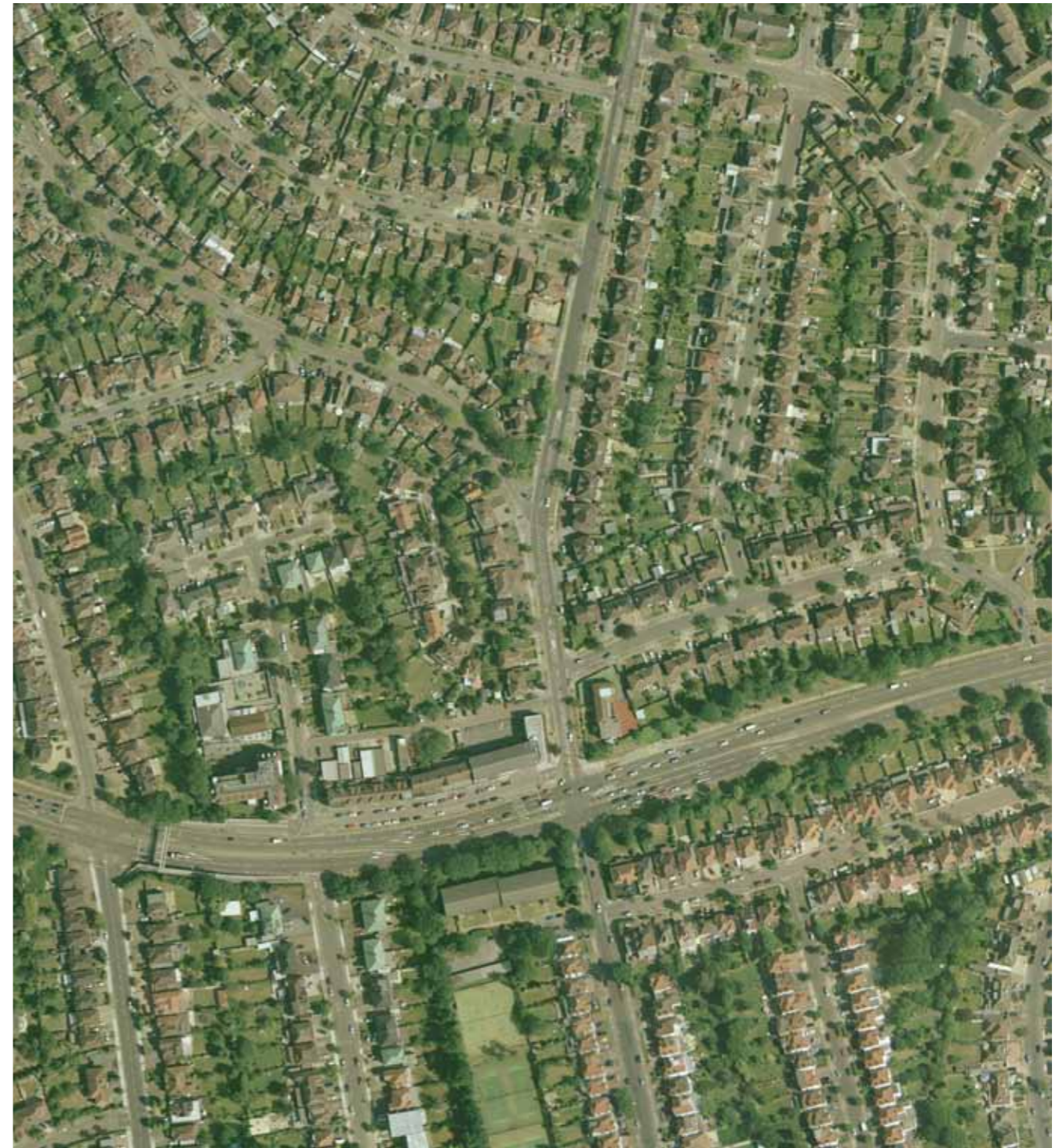
## *Landscape Character and Streetscape*

Most areas of suburban streets in the Borough have a broad, open street profile with medium to large front gardens. Many houses were originally designed with garages and a small formal front garden with lawns and/or ornamental planting. However, the primary use of front gardens today is parking. This is due partly to multiple car ownership and partly due to the conversion of garages to residential use. Many gardens have therefore been converted to include extensive areas of hard standing to accommodate this change. This has resulted in a harder, more urban street character than would have originally been intended when the houses were first designed.



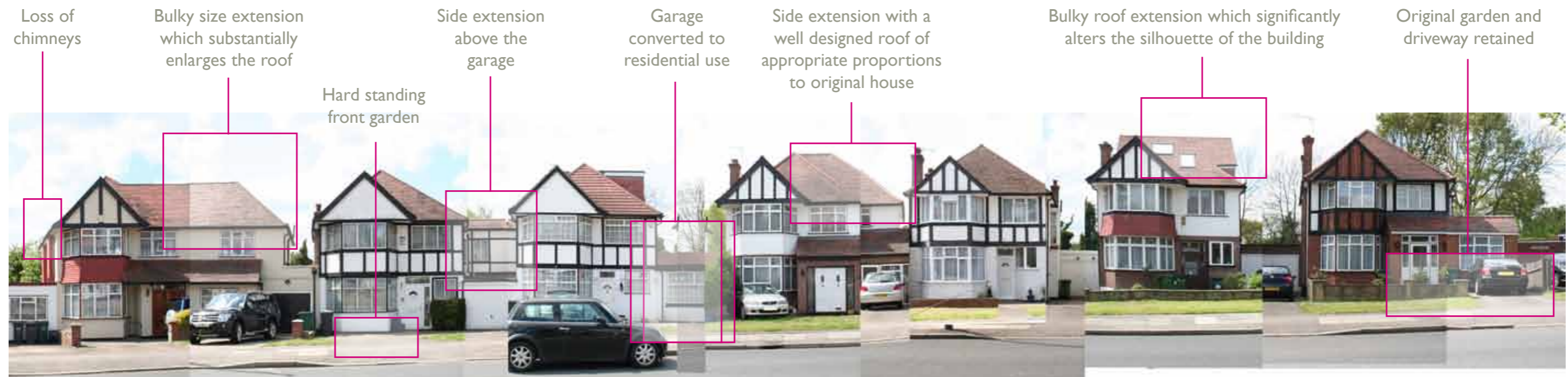
*Figure ground plan of typology*

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*Aerial view of typology*

# C - SUBURBAN



Photographic elevation of typical suburban street



Graphic Elevation of typical suburban street illustrating architectural rhythm

Many streets include a 0.5 - 1.5m wide mown grass verge separating the road and pavement and incorporating small street trees. In some areas the grass verge is replaced with intermittent street trees. Both verges and hedges are being gradually eroded by the increasing requirements for vehicle crossovers, reducing the amount of on street vegetation. The front boundary to properties is mainly formed by low brick walls of varying style and age.

Streetscape materials are typically macadam roads with a granite or concrete kerb and pavements surfaced in pre-cast concrete slabs or macadam. Private drives are surfaced in a wide variety of materials ranging from slab paving to modern concrete, concrete block and brick paving. Street furniture and lighting is mainly late twentieth century standard highway fittings.

# D - SUBURBAN TERRACE

## Introduction

As the name suggests, a key defining characteristic of this residential street typology includes the prominence of terrace housing. Suburban terrace streets can be distinguished from urban terrace streets (residential street typology E) by its lower density and more varied architectural style.

Across the Borough, suburban terrace streets are composed of buildings from a range of architectural styles and periods. Despite this borough-wide variation, individual streets retain a strong sense of architectural cohesion



Suburban terrace streets contain buildings associated with a variety of architectural periods and styles, including the interwar period. Individual streets typically have a strong sense of consistency in built form.

and consistency, as they are typically formed of buildings from single area. Perhaps the most distinctive form of architectural style includes housing inspired by the Garden City movement. Privately built houses from the interwar period are also common, and there are a few streets that contain buildings dating back to the Edwardian and Victorian periods. Suburban terrace streets enjoy a greater degree of enclosure than suburban streets, although building set backs remain large enough to accommodate off street parking.

## Density

The density of suburban terrace streets typically varies from 20 – 30 dwellings per hectare.

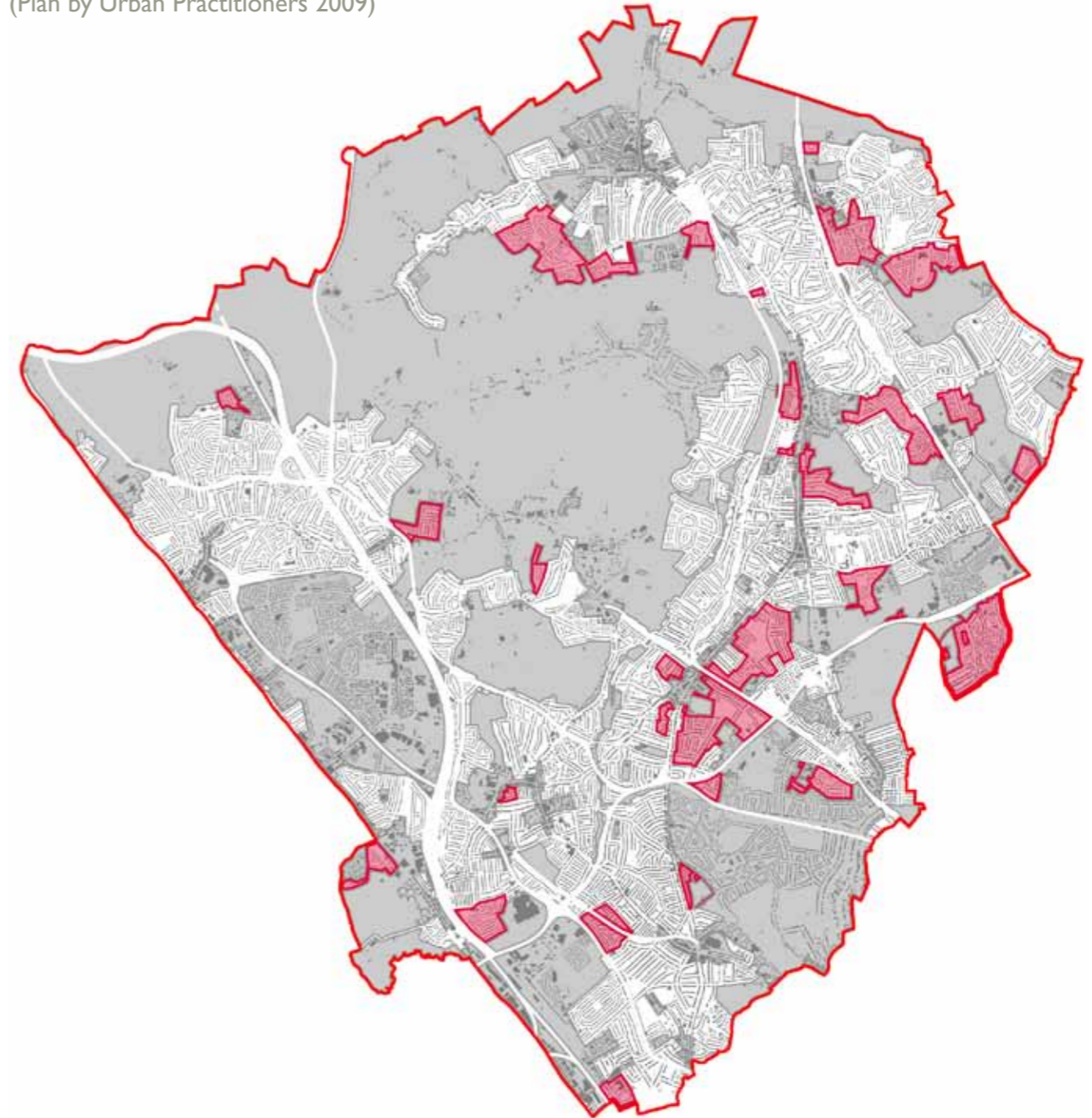
## Building Types and Units

Buildings along suburban terrace streets are terraced houses, the vast majority of which are two storeys high. Occasionally, some streets have houses that are three storeys in height.

## Enclosure

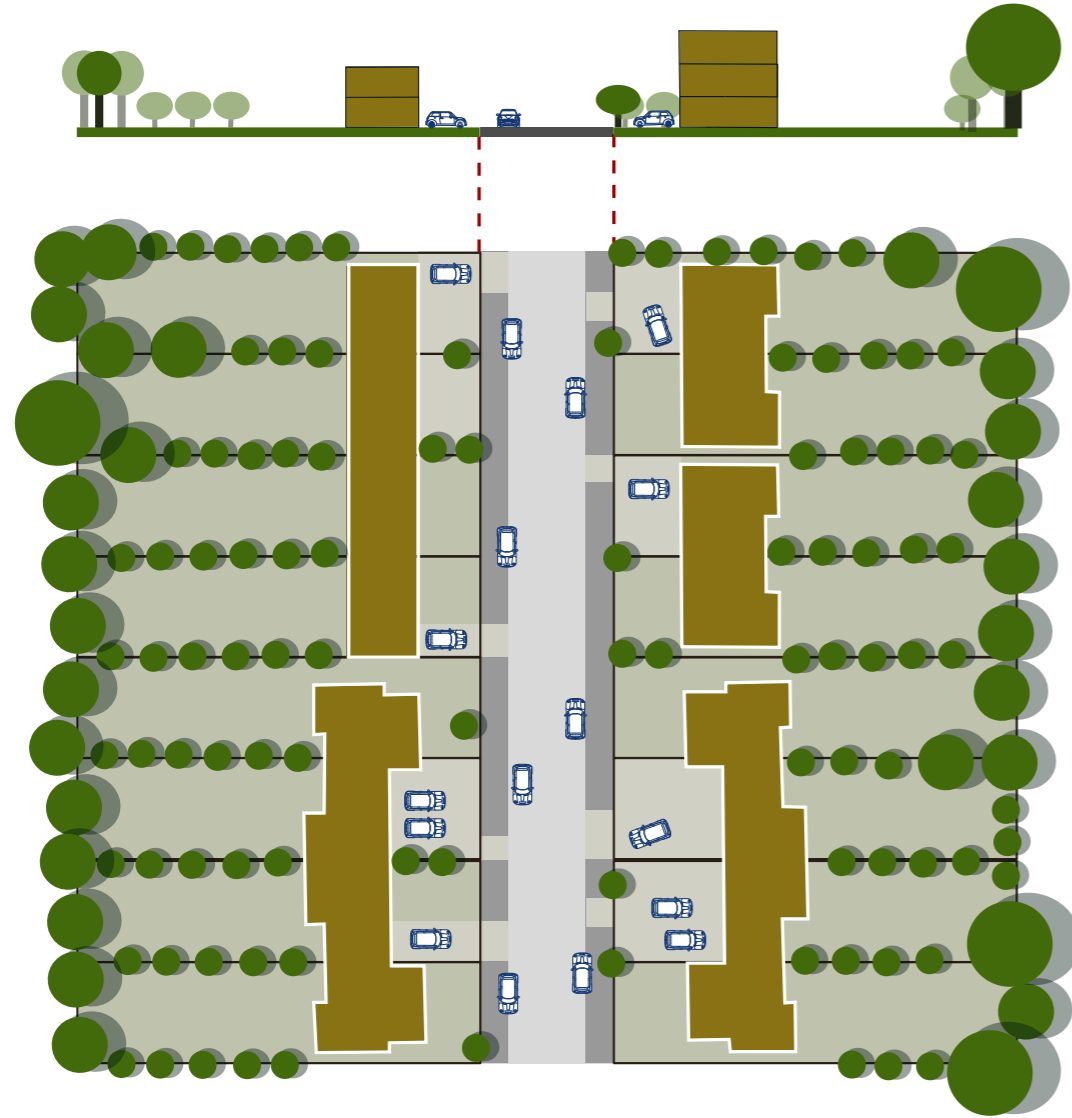
In similar fashion to suburban streets (secondary typology C), buildings on suburban terrace streets provide the primary enclosure to the street. The street space is generally wide, and varies between 12 – 18 metres in width. Plot widths are narrower than those found on secondary typology C, however buildings do retain a significant set back. Typically, the distance between building front and the plot edge varies from 4.5 to 7 metres. This provides limited space for on-plot parking.

## Suburban Terrace Streets (Plan by Urban Practitioners 2009)



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# D - SUBURBAN TERRACE

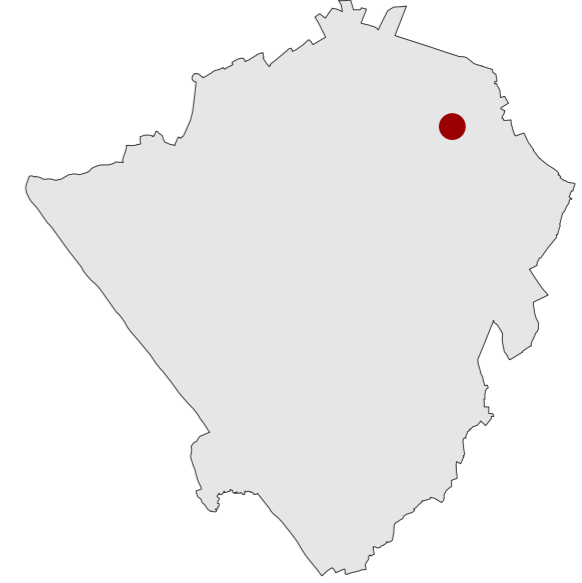
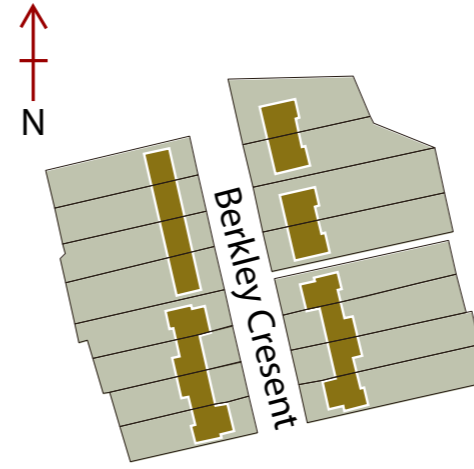


Street Width  
9 - 12 M

Set Back  
4.5 - 7 M

Plot Depth  
28 - 40 M

Plot Width  
6 - 10M



# D - SUBURBAN TERRACE

## *Landscape Character and Streetscape*

The street profile for suburban terrace areas in the borough is typically broad with moderate sized front gardens and some off street parking. The earlier examples of suburban terrace typically have broader street profiles with street trees set in a narrow grass verge separating the road and the pavement on both sides. In some later examples the profile is narrower with no street trees or grass verge (for example, Berkeley Crescent, EN4) and more prominent on-street parking.

A significant proportion of front gardens on suburban terrace streets have been paved over to accommodate off-street parking. Streetscape materials are typically macadam roads with a wide granite or concrete kerb and pavements surfaced in pre-cast concrete slabs. Street furniture and lighting is mainly late twentieth century standard highway fittings.

## *Architectural Style and Period*

Houses inspired by the Garden City movement tend to be flat fronted with few of the ebullient details common in private sector properties. Terraces are often arranged carefully as a group composition, with strong elements of symmetry often sitting within a larger planned estate pattern. They are generally smaller houses (compared to suburban types) in a terraced format. Individual properties tend to be squarer on plan with a wider frontage than earlier narrow terraced houses.



*Figure ground plan of typology*

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A significant proportion of houses on suburban terrace streets were constructed during the interwar period. Where as detached homes built during the same era retain a horizontal emphasis on suburban streets, the narrower plots and terrace form of houses of suburban terrace streets tend to have a more vertical emphasis of rhythm.



*Aerial view of typology*

# D - SUBURBAN TERRACE



Photographic elevation of typical garden city style suburban terrace



Graphic elevation of typical suburban terrace street illustrating architectural rhythm

Some streets identified as this typology are lined with houses dating back to the Edwardian and Victorian periods. They share a similar approach, to that of interwar housing, of terraced houses on relatively narrow plots with a similar degree of repetition. However, due to their more detailed building facades, Edwardian housing maintains a greater impression of vertical emphasis and rhythm along the street. The majority of housing from this period has been identified as residential typology E (urban terrace). The key distinguishing factors between houses on suburban terrace streets and those on urban terrace streets includes the greater building set back (which can accommodate on plot parking) and lower net densities.

# E - URBAN TERRACE

## Introduction

Urban terrace streets are predominately characterised by the architectural dominance of Victorian and Edwardian housing. Unlike large areas of London, Barnet has very limited areas of Edwardian and Victorian development, and most of it is located in the south and east of the Borough.

Streets are typically urban in character, dominated by on street parking and with minimal parking. They normally have a strong, coherent character due to the consistent and rhythmic architectural style and consistent street profile. An additional key physical characteristic includes the relatively narrow streets and limited building set backs.



Frontages have a strong vertical rhythm

## Density

Urban terrace streets represent some of Barnet's most dense residential housing. Whilst most suburban environments do not achieve densities of 30 dwellings per hectare, typical urban terrace streets have densities in excess of 35 dwellings per hectare.

## Building Types and Units

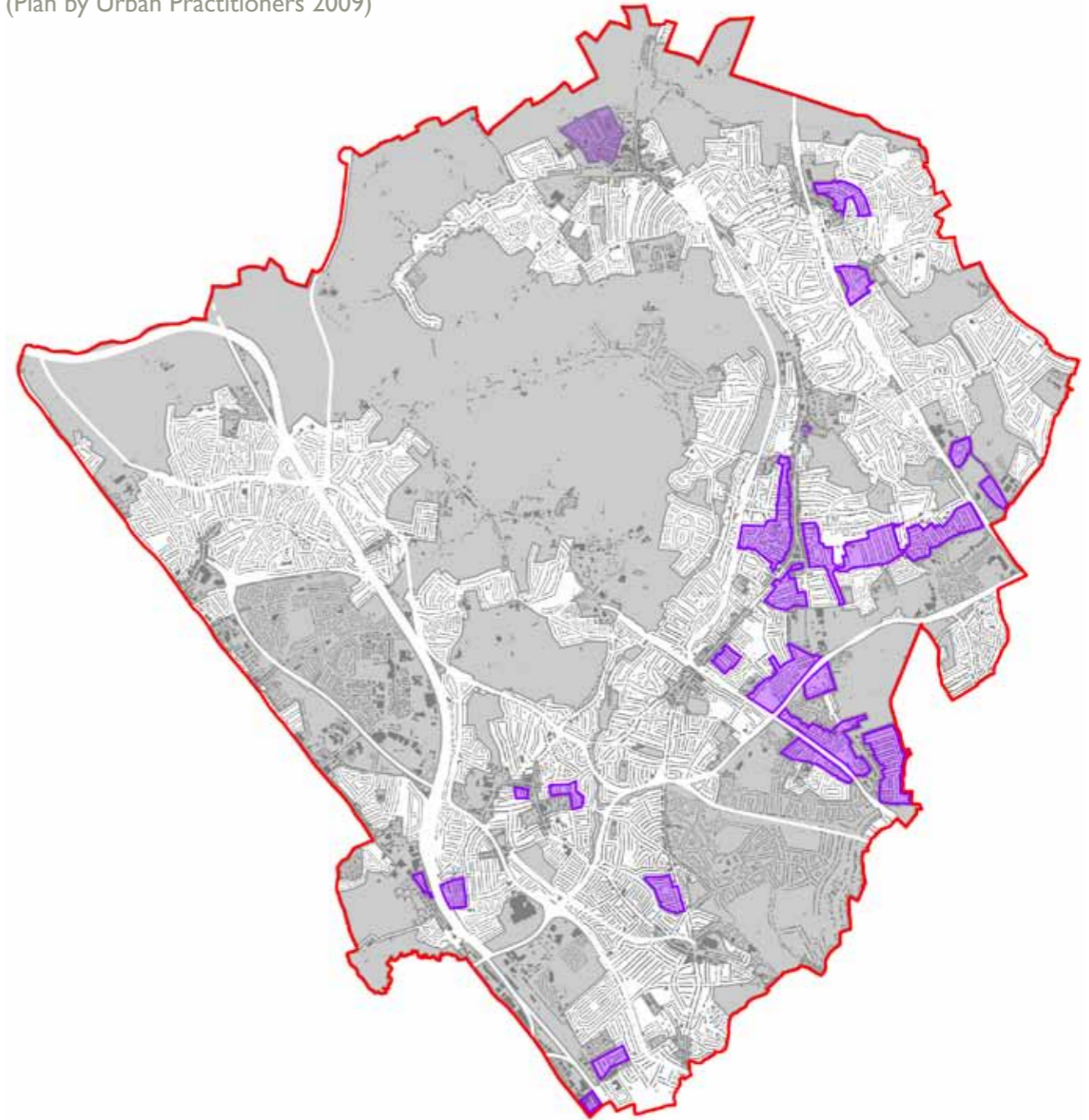
As its name suggests houses are terraced, and are of 2 – 3 storeys in height.

## Enclosure

Buildings provide the primary enclosure to the street in the urban terrace typology. Street widths are typically narrow, have a typical width of between 10 and 11.5 metres. Building plots are very narrow and there is limited building setback; the distance between the plot edge and building front varies between 1 and 3 metres. The narrow street widths and reduced building sets backs combine to emphasis the sense of street enclosure. Due to this lack in depth, there is no space for on plot parking and consequently on street parking typically dominates the streetscape.

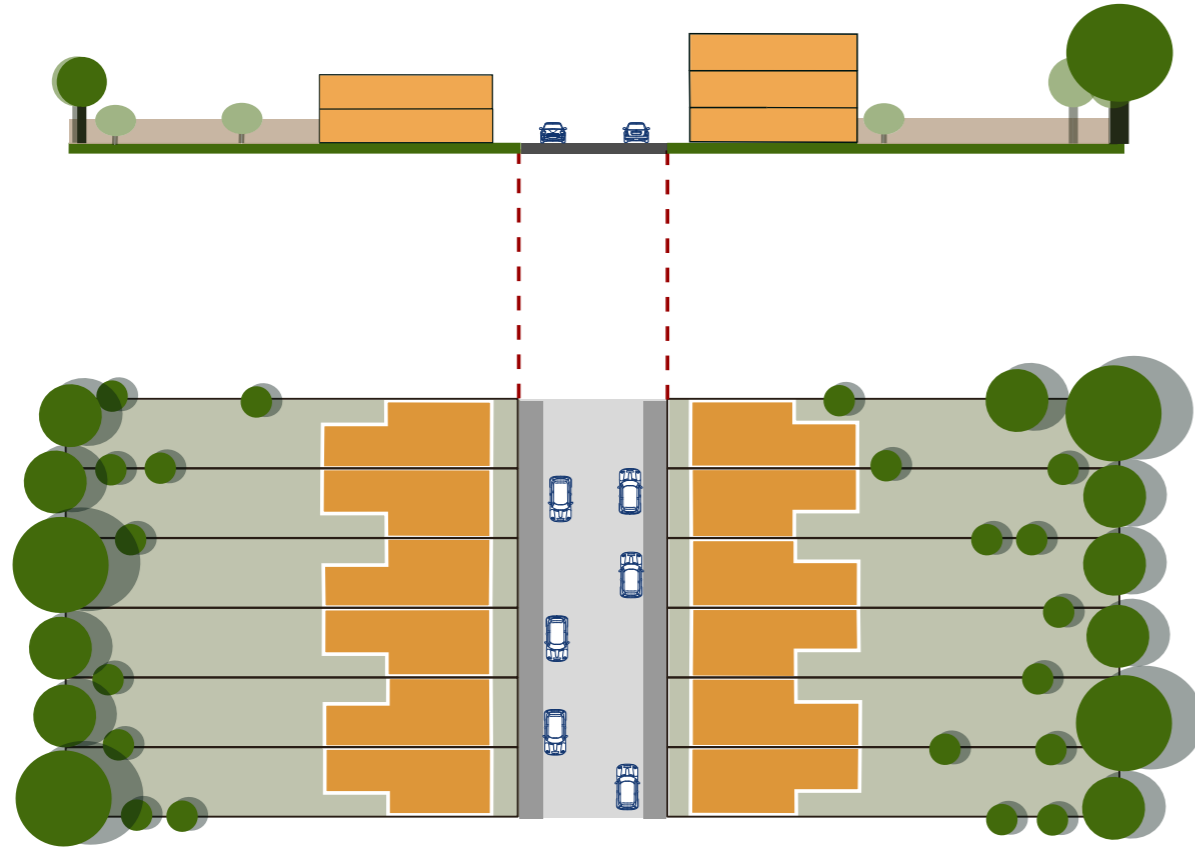
## Urban Terrace Streets

(Plan by Urban Practitioners 2009)



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# E - URBAN TERRACE

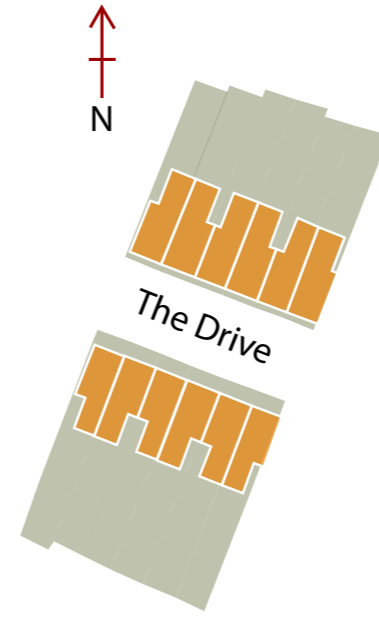


Street Width  
10 - 11.5 M

Set Back  
1.5 - 3 M

Plot Depth  
22 - 45 M

Plot Width  
3.5 - 5.5M





# E - URBAN TERRACE

## Landscape Character and Streetscape

Greenery in urban terrace streets is normally limited due to the shallow front gardens and narrow street width. However, street tree planting of ornamental varieties of small trees such as ornamental cherries, purple leaved cherry and silver birch is more common on the Edwardian streets where more space is available.

Front gardens are typically small and many (particularly the smaller Victorian front gardens) have been partly or fully paved but often incorporate some domestic ornamental planting. Planting varies from low and medium sized evergreen and deciduous shrubs to mixed herbaceous perennial and annuals. In most, there is insufficient space for trees or large shrubs. The small size of the gardens has also had the positive effect that there have been no conversions to off-street parking which has allowed the street profile to be maintained intact. Gardens and boundaries are typically well-maintained.

The original Victorian and Edwardian road and pavement surfacings of crushed stone and gravel have been removed and today all the carriageways on urban terrace streets in the borough are surfaced in macadam. Most retain the original wide Victorian/Edwardian granite road kerbs. Some also retain the original road gutter formed from three or four rows of granite setts. Pavements are generally surfaced in pre-cast concrete slabs and the roads are lit with standard late twentieth century highways light fittings. Boundaries to front gardens are typically low brick walls of varying age and style. They often have a hedge or shrub planting growing behind which forms a vertical extension to the wall.



Figure ground plan of typology

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Building set backs are considerably limited, as front gardens are not able to accommodate on plot parking



Aerial view of typology

# E - URBAN TERRACE



Photographic elevation of The Avenue



Graphic Elevation of typical urban terrace street illustrating architectural rhythm

## Architectural Style and Period

Urban terrace streets predominately consists of Victorian and Edwardian housing, typified by their regular grid street patterns and terraced form. The few examples feature the conventional narrow terraced form with a stepped rear elevation and unified front elevations. The repeated narrow properties give a predominantly vertical rhythm to the street, often emphasised by repeated chimneys and parapet walls along the roof. There is typically a high degree of repetition along a terrace, with buildings featuring repeating bay windows and porch details. The earliest houses are stucco rendered with sash windows and slate roofs, later giving way to buff or red brick by the turn of the twentieth century.

# F - FLATS

## Introduction

During the postwar period there was an increasing trend for large Victorian and Edwardian buildings to be bought up and demolished to be replaced by low rise apartment blocks, most typically flat roofed designs. This approach is seen to occur in concentrated areas along particular streets. These streets including Lyonsdown Road and parts of Hendon Lane, where selected for incremental redevelopment due the significantly large plot sizes, and the economic potential they present.

Although flats built during the postwar period are the most common type of block along these routes, there is a mixture of Victorian, Edwardian, interwar and modern buildings. The incremental development of these routes has created a distinct lack of uniformity in the built form; a key distinguishing characteristic of this typology.

## Density

Flats represent the highest density residential environments throughout the borough, typically ranging anywhere from 80 – 150 dwellings per hectare.

## Building Types and Units

Streets are lined with flats that have typical building heights of three to six storeys, either with flat roofs or pitched roofs.

## Enclosure

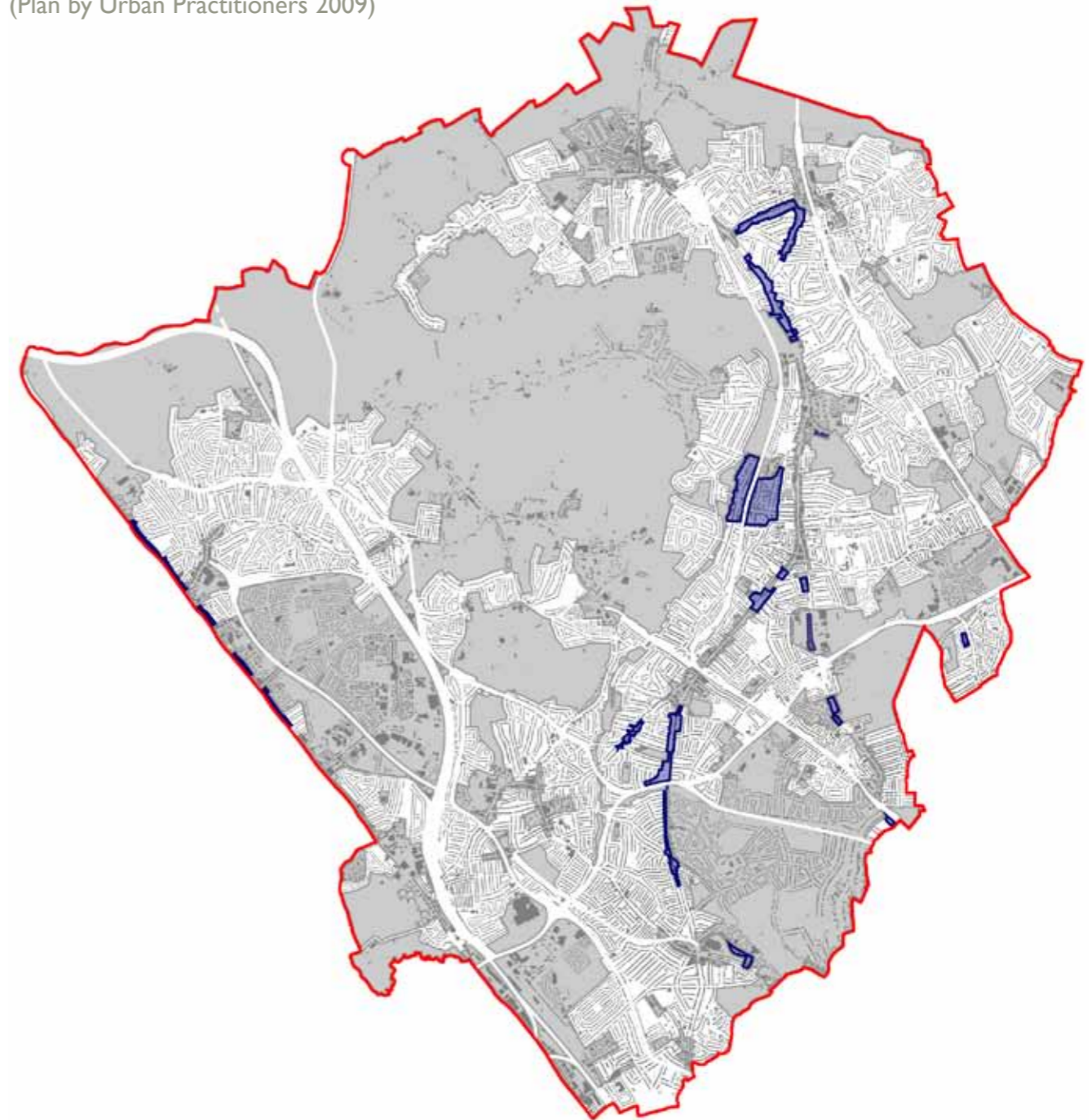
Buildings provide the primary form of enclosure to the street, as street widths can vary from anywhere between 4 and 13 metres. Buildings are well set back, as the distance between building front and plot edge ranges from 5 to 17 metres.



Modern flats on Hendon Lane

## Flats

(Plan by Urban Practitioners 2009)



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# F - FLATS

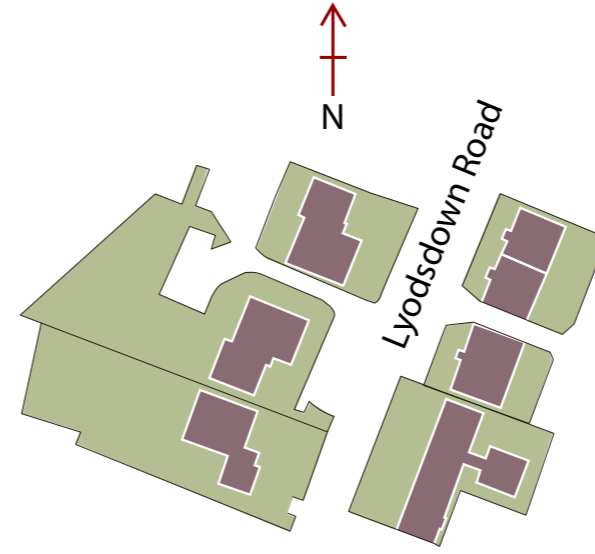


Street Width  
4 - 13 M

Set - Back  
5 - 17 M

Plot Depth  
30 - 90 M

Plot Width  
22 - 34 M



# F - FLATS

## *Landscape Character and Streetscape*

Modern apartment blocks typically have fringes of landscaped areas of mown grass and mixed shrub planting (mainly evergreen species). Parking is generally arranged to the rear or side of these blocks in small car parks surrounded with small areas of mixed ornamental tree and shrub planting.

Where older Edwardian and Victorian properties have been sub-divided into multiple occupancy, front gardens have often been converted to hard standing to accommodate the increased demand for several parking spaces.

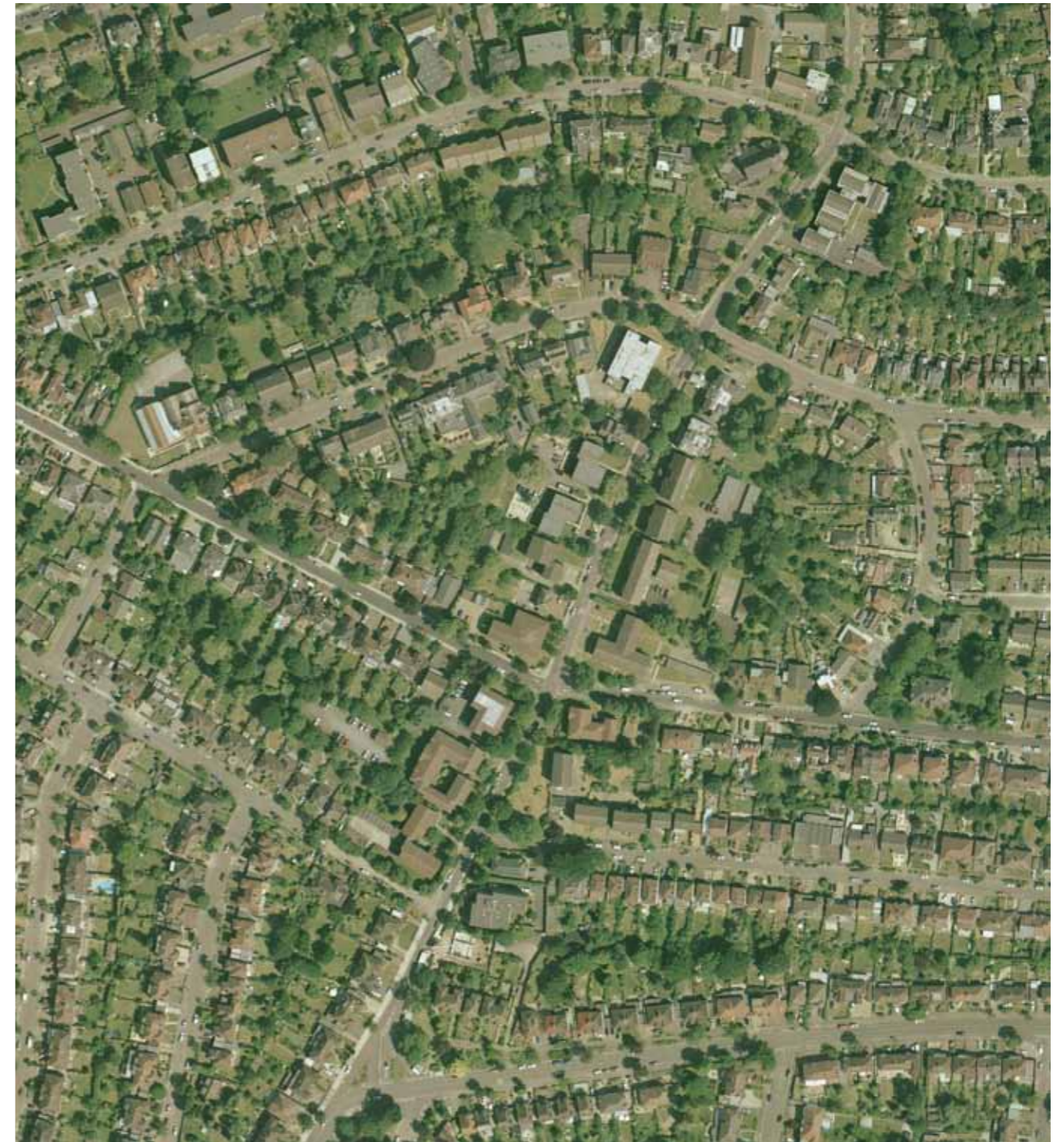
Vegetation along the routes varies considerably in species and maturity. Many routes include mature trees, irregularly spaced in front gardens and with a wide range of species including large trees such as Horse Chestnut, Oak, Copper Beech and Cedar. Smaller varieties such as purple leaved Cherries (*Prunus cerasifera* var. *atropurpurea*) and Rowan (*Sorbus* sp.) are also present, planted as street trees in pavements. Where trees and garden planting are large and mature, these convey an attractive leafy character to the area.

Streetscape materials are typically macadam roads with a wide granite kerb and pavements surfaced in pre-cast concrete slabs. Crossing points are frequently provided in the form of pedestrian lights or zebra crossings due to the significance of these roads in the local street network.



*Figure ground plan of typology*

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*Aerial view of typology*

# F - FLATS



1960s flat roof development

Rear of plot redevelopment

Original late Victorian semi detached villa, substantial in scale and with robust detailing

Replacement for a single plot, with an archway through to provide access to parking at the rear

Replacement for a double plot with large area of surface parking at the front

Photographic elevation of typical flat redevelopment



Period buildings have been converted to flats



Flats are well set back from the street



Postwar redevelopment of Lyonsdown Road

## Architectural Style and Period

The architectural style and period evident on streets with flats is frequently wide ranging, due to their incremental redevelopment. Routes often includes purpose built apartment blocks alongside period buildings that have been converted to flats.

Purpose-built flats, as opposed to period conversions, are generally bulkier and more monolithic than even than the largest single dwellings and therefore exert a strong physical presence on their streets. The most successful are perhaps those that do not over-stretch the traditional domestic model and instead follow their own architectural expression.

Less successful tend to be the many blocks of flats that supplant one or more houses within existing street frontages, and often struggle to relate convincingly to the character and scale of their neighbours.

# SUMMARY

The table below provides a summary of the secondary typologies, and is analysed against the criteria used to define each typology. The adjacent plan summaries the secondary typologies.

	Density (Dwellings per hectare)	Density (Dwellings per hectare)	Density (Dwellings per hectare)	Building Types	Heights (Storeys)
<b>Linear Rural</b>			2 - 10	Detached houses	1 - 3
<b>Suburban Periphery</b>			10 - 15	Detached houses	2 - 3
<b>Suburban</b>			20 - 30	Semi-detached houses	2 - 3
<b>Suburban Terrace</b>			20 - 30	Terraced houses	2 - 3
<b>Urban Terrace</b>			37 - 50	Terraced houses	2 - 3
<b>Flats</b>			80 - 150	Street facing flats	3 - 6







# SUMMARY

Enclosure	Street Widths	Setbacks (building front from plot edge)	Landscape character / Streetscape	Architectural Style / Period	Photograph
Street space is enclosed by trees, hedges and other vegetation	Narrow Street space / lane, with little or no pavement (15 - 20 metres)	Buildings are well set back (8 - 40 metres)	Typically leafy and rural, prominent vegetation with mature native trees and hedgerows.	Varied	
Street space is enclosed primarily by vegetation, as buildings provide secondary enclosure	Wide street space with pavement (11 - 13 metres)	Buildings are well set back (8 - 14 metres)	Leafy and exclusive character, as parking is well integrated into large front gardens. Front gardens contain an array of vegetation.	Varied	
Buildings provide primary enclosure to street.	Medium to wide street space, often with generous pavement (12 - 18 metres)	Buildings are well set back (4 - 9 metres)	Broad, open street profile with medium-sized front gardens primarily used for parking. Small trees evident on many streets.	Typically buildings from the interwar period	
Buildings provide primary enclosure to street.	Medium to wide street space with pavement (9 - 12 metres)	Buildings medium to well set back (4.5 - 7 metres)	Medium-sized front gardens primarily used for parking. Small trees evident on many streets.	Typically buildings from the interwar period, and occasionally late Edwardian. Houses influenced by Garden City movement also present.	
Buildings provide primary enclosure to street.	Narrow street space with pavement (10 - 11.5 metres)	Limited set back (1.5 - 3 metres)	Narrow street profile with shallow front gardens. Streets are dominated by cars with little room for vegetation.	Victorian and Edwardian buildings	
Buildings provide primary enclosure to street.	Wide street with pavement (4 - 13 metres)	Buildings well set back (5 - 17 metres)	Vegetation along the routes varies considerably in species and maturity. Front gardens have often been converted to hard standing	Typically post war blocks or Victorian and Edwardian mansions converted into flats	

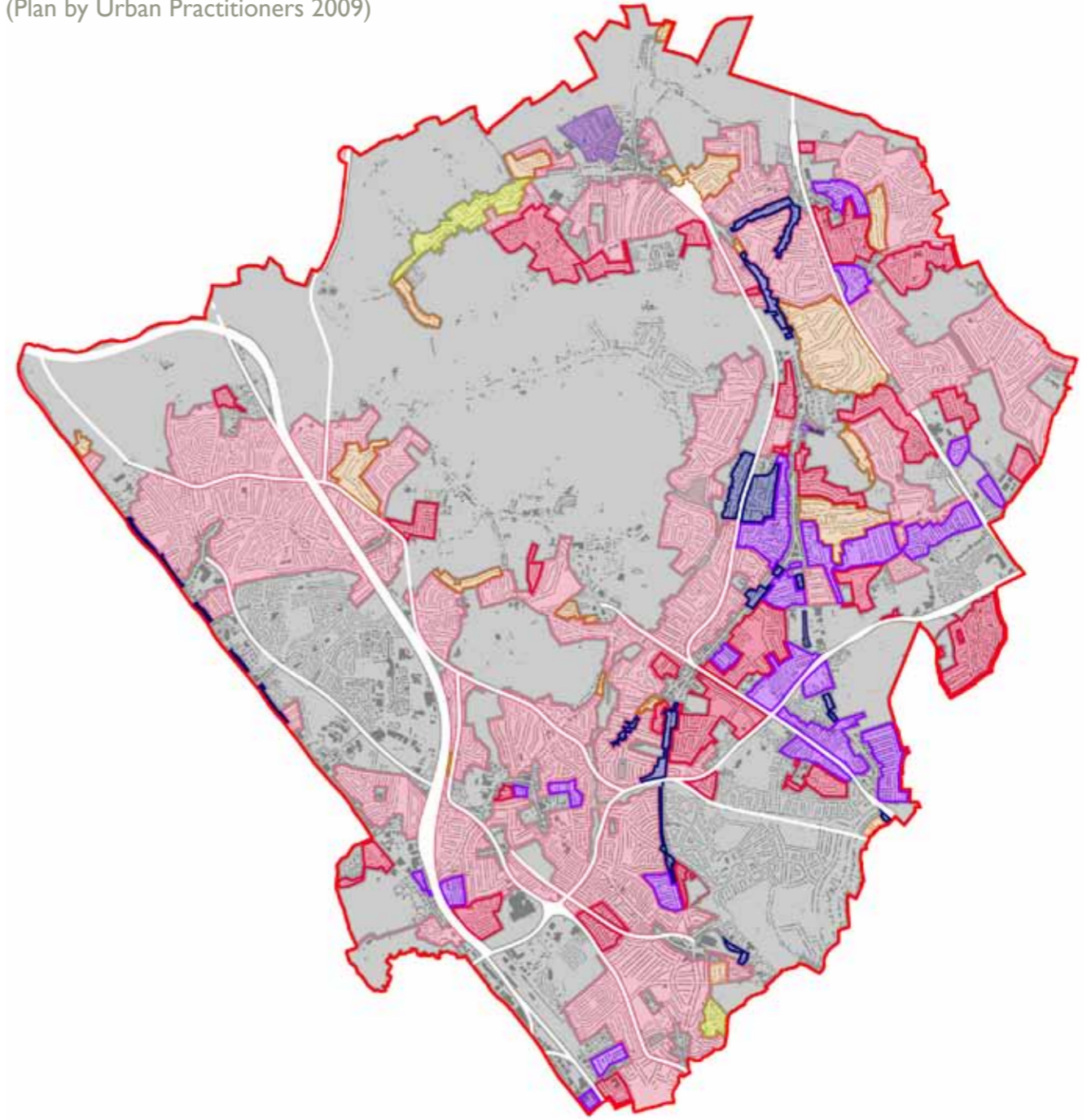


# SUMMARY

The plan illustrates the Borough wide makeup of secondary typologies, as residential streets have been further subdivided. When combined with primary typologies, including box development, campus development, town centres, and residential estates, this analysis will form the basis the identification of Borough wide character areas.

KEY	
	A - Linear Rural
	B - Suburban Periphery
	C - Suburban
	D - Suburban Terrace
	E - Urban Terrace
	F - Flats

**Secondary Typologies**  
(Plan by Urban Practitioners 2009)



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